



HUBBARDS COMMUNITY PLAN

Prepared by
UPLAND
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Aspotogan Heritage Trust

The Hubbards area businesses, community groups, and community members

Nova Scotia Department of Public Works

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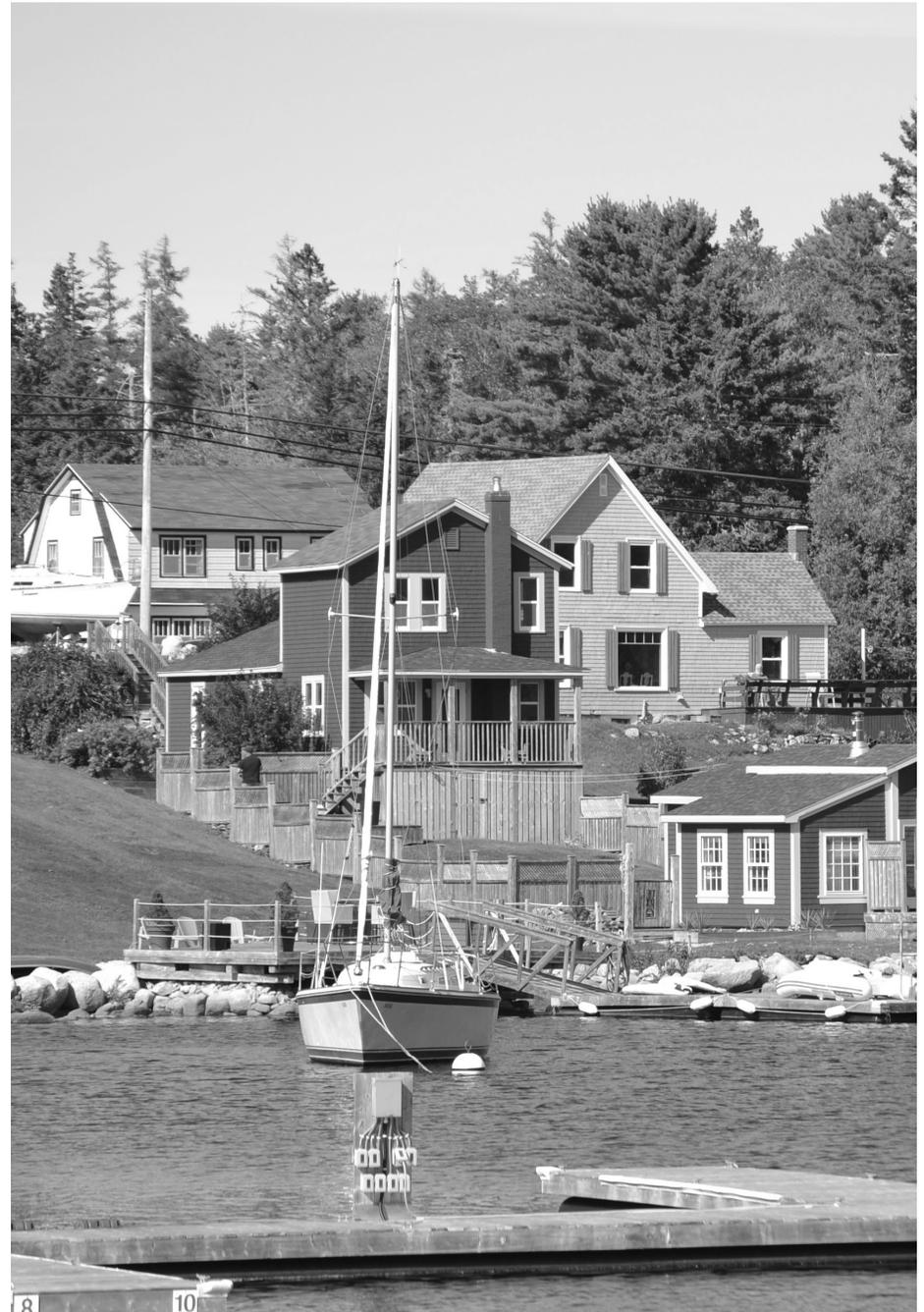


Table of Contents

I. Project Background and Public Dialogue

1. About the Community Plan	05
2. Initial Consultations	10
3. Formative Public Engagement	14
4. Engagement on Draft Plan	32

II. Community Analysis

5. Hubbards Facts and Figures	42
6. Community Development Vision	55
7. Connections and Big Moves	74
8. Streetscape Inventory	80

III. The Plan

9. Plan Principles	104
10. Hubbards Loop + Placemaking	106
11. Cove Connections + Sidewalks	131
12. Interim Projects	141
13. Development Policies	147
14. Implementation	157



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PART I

Project Background and Public Dialogue





1. ABOUT THE COMMUNITY PLAN

1.1 PROJECT BACKGROUND

Hubbards is a beautiful seaside village with a library, a school, waterfront amenities including a marina and sailing club, a skate park, sports fields, museums and a wide range of businesses that cover most basic needs of daily life. The community is beautifully situated between three popular beaches and has access to a long distance trail, which wraps around Hubbards and provides off-road connections to places between Halifax, Lunenburg and beyond.

One of the biggest assets of the community is that most of these amenities are—at least theoretically—

within walking distance of each other. However, while the distances are small, there are currently many barriers to Hubbards being a tightly-knit, walkable community. This discrepancy between what the community is and what it could be was one of the main drivers for this project.

Seeing the vast potential of the possibilities, residents from the area began meeting and talked about needed improvements. The resulting Hubbards Streetscape Project (HSP) is a group of Hubbards area residents and volunteers who aim to create a safer, more active and

convivial community for residents. HSP began forming in 2019 and is strongly aligned and focused. The group has strategically partnered with the Aspotogan Heritage Trust to be a community and administrative partner, including in the creation of this community plan.

Surprisingly, no level of government has taken the initiative yet to create a plan for the community of Hubbards as a whole. This circumstance can be possibly attributed to the fact that Hubbards faces some jurisdictional overlaps. The Halifax-Lunenburg county line

effectively divides Hubbards in two. Municipal governance is therefore split between the Halifax Regional Municipality and the Municipality of the District of Chester. Each of the municipalities has a set of plans for Hubbards, but only up to the respective boundary line. In addition, nearly all roads in the community are provincially owned.

The Hubbards Streetscape Project has therefore hired UPLAND Planning and Design to create a Community Plan that breaks up jurisdictional boundaries and is centred around the community's needs.



1.2 INTENT OF THE COMMUNITY PLAN

While concerns over pedestrian safety were the main impetus for the forming of the Hubbards Streetscape Project, the group soon realized that the question of "*How do we want to move around in our community?*" is closely related to another question, which is "*How do we envision our future community to look like?*".

There are many reasons to believe that Hubbards may experience a boost of growth over the next decade. Nova Scotia is experiencing a level of population growth not seen in decades. Halifax, as the epicentre of that growth (and housing shortage), is only a 30-minute drive away. The Province is about to complete the last section of the Highway 103 twinning, which will furnish the residents of Hubbards with a continuous four-lane divided highway to the provincial capital. This will make commutes more comfortable, just as self-driving cars and the increasing share of home office hours make close proximity between homes and employment less and less relevant. With these factors in mind, it appears plausible that questions of growth and development in Hubbards are relating to 'when' rather than 'if'.

These considerations are closely intertwined with active transportation planning.

It is important for future development to create a built environment that supports active living and community building. After more than half a century of the suburban experiment in North America, we know that these community characteristics cannot be taken for granted. At the same time, active transportation connections and safety improvements need to target areas where most growth is expected to occur. Both aspects go therefore hand in hand to form the basis of this community plan.

The goals of this plan are therefore to advance the following causes for Hubbards:

SAFE AND ACCESSIBLE STREETS

The main challenge for residents is the lack of connectivity between all elements of the community. Residents walking between key buildings and amenities do not feel safe, especially persons living with accessibility needs. In order to leverage funding for infrastructure improvements, the community needs a structured analysis of current and future pedestrian movement, including an identification and prioritization of safety improvements.

'MAINSTREETING'

Creating a comprehensive vision for Hubbard's main street named 'St. Margaret's Bay Road'¹ is closely related to the previous point. Research suggests that drivers are generally more likely to react to visual cues from the environment than to speed limits. The looks of a main street including street widths, tree planting, signage and design of surrounding buildings are among the factors that affect drivers' behaviour and travel speeds.

Buildings located close to the road with large windows and decorative elements completely change the impression of a street and indicate the core of a community. This process of transforming rural or suburban roads—sometimes referred to as 'mainstreeting'—is therefore another anchor of the community plan.

DEVELOPMENT POLICIES

The current Municipal Planning Strategy and Land Use By-laws on both sides of the municipal

¹ Note: Within the Halifax Regional Municipality, the main street of Hubbards is named St. Margaret's Bay Road. After crossing the municipal boundary into the Municipality of the District of Chester, the road is simply addressed as Highway No.3. However, for ease of reference this Community Plan will refer to St. Margaret's Bay Road for all of Hubbard's main street.

boundary line provide little guidance for development in Hubbards, as these documents apply rules with a broad geographic focus. The Community Plan therefore needs to be written in a way that helps planners of both municipalities to adopt recommendations into official policy documents.

COOPERATION

There are stretches of provincial highway in Nova Scotia which already function as 'main streets', whether in Tatamagouche, St. Peter's or Port Hawkesbury. The Nova Scotia Main Streets Initiative advanced by the Nova Scotia Federation of Municipalities (NSFM) adds to this momentum.

This Community Plan is expected to strengthen cooperation with stakeholders at all levels of government, to draw from these resources to advance the goals of the Hubbards Streetscape Project, and finally to give back to the wider community through the establishment of a good practice precedent of community planning across organizational boundaries.

1.3 PROJECT HISTORY AND PRELUDE

The community of Hubbards has devoted much time and effort to community planning. A recap of the extensive community consultation and strategic planning efforts carried out to date is therefore a logical starting point for renewed planning activity. A review of past conversations will ensure that the new plan is sensitive to needs and wants of the community.

Hubbards has been slowly growing and developing its own unique charm since 1757. In 1994, a big shift occurred in the community when the Federal Government announced the closing of the Canadian Forces Station Mill Cove, a large economic driver in the area. Acknowledging the significant impact this decision would have on the local community and its economy, the

government awarded the community a \$2 million fund – administered by the newly formed Aspotogan Heritage Trust (AHT). Not long after their founding, the AHT purchased the Hubbards Barn property, as well as the Community Waterfront Park. These properties have become local community hotspots, with the Barn hosting the weekly farmer's market

and providing a space for community events, as well as private events. The AHT also owns the building where the Trust operates and hosts the local daycare in the space next door. The AHT continues to fund

*“You can’t really know where you are going,
until you know where you have been.”*

– Dr. Maya Angelou



various community and development projects throughout the region and has been a major contributor to strategic planning projects since its inception.

THE C.A.S.T. PROJECT

The community, along with government, community and academic partners have been working towards a comprehensive planning strategy since 2010 with the “Creating a Stronger Tomorrow” (CAST) project. This project was “a community consultation process designed to help the region develop a community driven approach to establish and maintain a sustainable economy for its people” (Public Presentation & Review, January 2011). Though operating on a larger geographic scale than the Hubbards Streetscape Project, CAST set out a vision for the future that outlined strategic priorities and goals. The core values that emerged from the extensive, year-long public consultation were: inclusion, mutual respect and support (between the communities of the Aspotogan Region), family orientation, environmental sustainability, quiet enjoyment, tradition/heritage, health, and education.

THE D.A.R.T. INITIATIVE

CAST was considered Phase 1 of the Strategic Planning process. Phase 2 began in 2012, known as Developing the Aspotogan Region Together (DART). Three strategic priorities emerged for this phase:

- + social capital
- + community planning and design
- + economic development.

A planning and design working group was formed to focus on cooperative planning policy development with local planning departments and advisory groups to ensure policies support the community vision laid out by the CAST project. The group also focused on creating strategies to retain youth and attract families, develop an active transportation plan, and identify opportunities for preserving environmental assets like trails, viewpoints and coastal areas. It is unclear from background documents if these groups were formalized.

In 2014 a group of Acadia University students completed a Strategic Plan for Hubbards building on the work done through the CAST and DART projects. The students identified the following strategic priorities for the

community moving forward: identity development, social services and recreation (primarily focused on youth), sustainable local economy, and active transportation.

HUBBARDS STREESCAPE PROJECT

Though no comprehensive planning strategy has been legally adopted to date, it is clear that the community remains committed to seeing its vision for a better future come to fruition. New families are moving to Hubbards, recreation areas for youth have been developed (e.g. the skate park) and a strong sense of community has been forged through investment in “third places” like the Hubbards Barn and the Community Waterfront Park.

The formation of the Hubbards Streetscape Project in 2019 represents a tipping point in the momentum for change. Residents were frustrated by the unsafe walking and cycling conditions in their community and decided to self-organize in an effort to do something about it. A Facebook group has turned in to a movement to push for the the long-standing vision for a safer, more accessible and beautiful Hubbards.





2. INITIAL CONSULTATIONS

2.1 HSP CONSULTATION EFFORTS + SURVEY RESULTS

The Hubbards Streetscape Project reinvigorated the public consultation and engagement efforts in 2020 through the creation of a Facebook page, website, local door-to-door canvassing, media interviews and a survey.

Canvassing took place in the fall of 2020 and was completed by a committee member and volunteers. Approximately 200 homes were reached and each was given an HSP information package, consisting of an info card, sticker and paper copy of the survey. A map of the area covered

can be found below in Figure 1. The overall reception from residents was enthusiastic, with only one person showing skepticism (who eventually warmed to the idea after conversation).

The survey was open to the public, consisted of 6 questions, including an opportunity to provide comment at the end and had 151 respondents. The first question asked residents to identify specific locations in the community that they want to become safer. The results are mapped and identify the following intersections and sites as the key

areas that need to be made safer:

- + The Library/Hubbards Cove Coffee street crossing
- + The Sailing Club crossing
- + St. Margaret's Bay Road /Route 329/Shopping Centre intersection
- + Shatford Memorial School

In addition to these specific locations, the stretch of St. Margaret's Bay Road between the library and the school was identified as a significant safety concern by survey respondents, as well as the section between the

school and the recreation centre.

The second questions asked residents to identify areas where they would like to see investment to create better public space for gathering, socializing, etc.

Respondents said that the Community Waterfront area was the place they would like to see the most investment, as well as the stretch of St. Margaret's Bay Road between the library and bridge.

When asked how they would feel about new residential and commercial



Figure 1: Canvassing route map

development in Hubbards guided by a community plan that is informed by community input, residents responded overwhelmingly with “very positive” (62%) and “positive” (37%), with only one respondent answering that they felt “very negative.”

When asked how they would feel about new residential and commercial development without a plan to guide growth in the area (a “business as usual” approach), the response was more mixed, but the majority felt “negative” (42%) or “very negative” (36%), with 17% responding they felt “positive” and the remainder responding they felt “very positive” about a continued organic approach to development. The lack of a “neutral/ no opinion” option for the Likert scale on both of these questions may mean that not all perspectives are being accurately represented.

The fifth question in the survey asked residents what they would most like to see a community plan accomplish and were encouraged to include more than just one idea or thought. Upon analyzing the survey respondents’ answers, a number of key themes emerged.

Major themes, which were mentioned twenty times or more, were safety (91 mentions), active transportation (85

mentions), and business development (20 mentions). Secondary themes that emerged were greenspace/street beautification, community identity, accessibility and inclusion, recreation, and increased housing options.

MAJOR THEMES

The major themes of safety and active transportation are to be expected given the primary focus of the HSP committee to date and the amount of emphasis that has been placed on these issues in the community. It is clear that this is a top priority for the community plan, with opportunity sites

clearly defined by the earlier survey questions as shown in the Appendix.

Respondents are keen to see road shoulder repairs and upgrades, or sidewalks added to high pedestrian/cyclist activity areas. Sidewalks were the primary desire, with a well-connected network that allows residents and tourists to access the shops, services and public spaces in the centre of the community.

Sidewalks are also a priority along the main street around the school, library, and daycare. It is clear from the survey results that the safety

of the community’s children is paramount to this plan. Respondents were also adamant about speed limit decreases in the community, as this is currently a huge safety concern.

The other major theme that emerged was business development. Survey respondents were keen to see small business remain in Hubbards, and for there to be more businesses operating in the community year-round. Respondents also mentioned design guidelines for new business development to ensure the character of the community remains intact.

There was also a desire to see investment in existing businesses that would allow for cosmetic upgrades, especially along St. Margaret’s Bay Road, to create a welcoming atmosphere encouraging people to stop along St. Margaret’s Bay Road .

SECONDARY THEMES

Street beautification was another theme that emerged from the survey results with respondents wanting to see more investment in the aesthetic appearance of the main street area. Suggestions included planting trees, adding flower beds and incorporating infrastructure like lighting and benches. Some residents would like to see more accessible public washroom

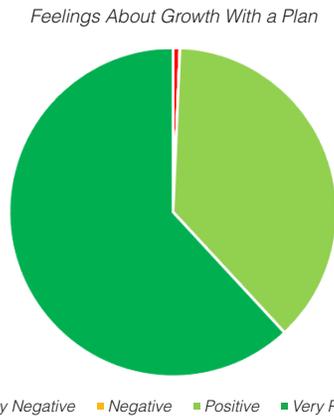


Figure 2: Results from Question 3 (How would you feel about new homes and businesses in Hubbards if residents could shape growth with a plan?) from HSP public survey.

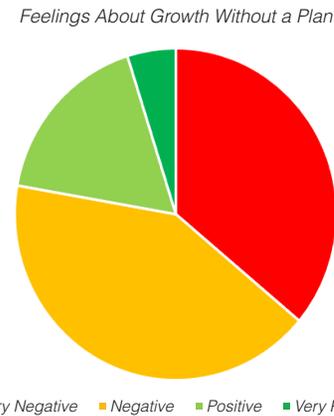


Figure 3: Results from Question 4 (How would you feel about new homes and businesses in Hubbards if the community had no plan?) from HSP public survey.

facilities as well. There was also desire to see existing greenspace maximized 'through professional design' to make these spaces more enjoyable and useable by the community and tourists alike. Respondents noted that investing in the existing sites with nice views would be especially welcomed.

Community identity was mentioned 15 times by survey respondents. They wanted to ensure that the “small town flavour” (as one respondent put it) remained intact and that growth areas for both commercial and residential development be thoughtfully considered. Respondents felt strongly that preserving and enriching the character of the community with context-specific design was very important to them. They want 'Hubbards to feel like Hubbards', with nods to its rich history and local culture, and not a copy of other communities in the region. That being said, Mahone Bay was mentioned by multiple respondents as an example of good community design where inspiration could be drawn for the Hubbards Community Plan.

Tied very closely to active transportation and safety, accessibility and inclusion was another secondary theme that emerged from the survey results. Respondents are keen to

see a community plan that ensures Hubbards is accessible for all ages and all abilities. This means wide sidewalks to accommodate wheelchairs and strollers and infrastructure that supports use by all residents (again, public washroom access and benches were mentioned as important pieces of the accessibility puzzle).

Increasing recreation opportunities in the community was mentioned by 9 survey respondents. Some suggestions included:

- + A dog park
- + Accessible play area for small children and those with special needs
- + Community waterfront play area
- + Community waterfront sitting area
- + Tennis & pickleball courts
- + Community swimming pool

The final theme that emerged from the survey was increased housing options. Respondents wanted to see more affordable housing opportunities for low-income or fixed-income residents, and more small housing options. There was also a desire to see more accessible housing options to accommodate older adults with decreased mobility. A few respondents

mentioned they want to see future residential growth limited and opposed any high-rise development (some citing two-storey limit).

GAPS

The community survey was an excellent starting point for engagement. Key locations for active transportation infrastructure, as well as key themes have been identified. Questions remain about what residents would like to see in the community plan.

- + Where would the community like to see residential and commercial development take place?

- + What kinds of housing options would residents like to see more of and where should they be located in the community?
- + What are the key characteristics of Hubbards that must be incorporated into streetscaping to project the community's identity?
- + What does street beautification look like in Hubbards? How will it be maintained?
- + How can public spaces be made more inviting for the community and for tourists?
- + Whose voices are missing from the conversation so far?

Q5: Thematic Analysis

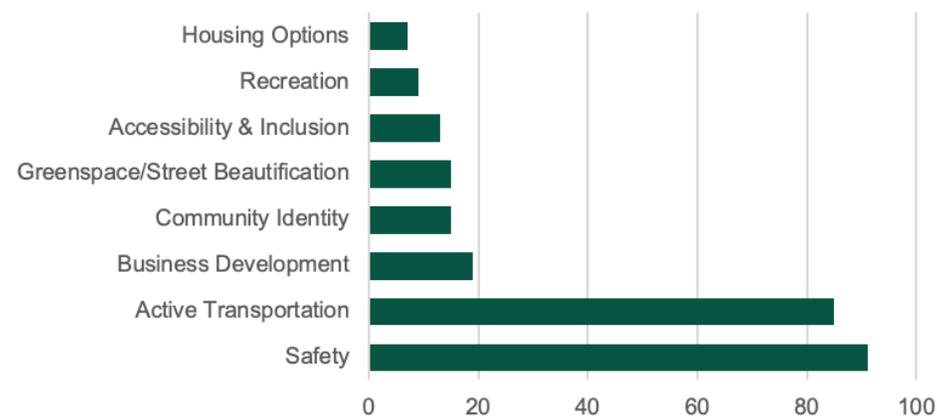


Figure 4: Responses from Question 5 of the HSP Survey (What would you most like to see a plan accomplish? More than one answer allowed) based on number of mentions by respondents.



3. FORMATIVE PUBLIC ENGAGEMENT

3.1 INTRODUCTION

The months of May and June 2021 were dedicated to public consultations on the Hubbards Community Plan. Unfortunately, the timeline of this project meant that these engagement activities had to occur at the peak of the third wave of the COVID-19 pandemic in Nova Scotia, which ruled out any in-person activities.

However, at this point in time, a large share of the community was well-versed with online meeting tools. Groups not available through the online meeting platform were reached by phone for interviews, which resulted in a good level of participation and inclusion overall.

The results from the many conversations with residents, community groups and stakeholders are an essential consideration for designing streetscape improvements and developing policy proposals.

The engagement resulted in general themes which have emerged from the conversations, but also offered direct and pragmatic advice for design solutions in particular locations of the Hubbards Community Plan project area.

3.2 OBJECTIVES

The objectives of this consultation phase were to:

- + Integrate community participation in the planning process through inclusive and meaningful engagement opportunities.
- + Share knowledge with Hubbards community members about the process of developing a plan, and clearly illustrate the scope of what a community plan can accomplish and how it can benefit the people of Hubbards.
- + Collaborate with local community representatives, governments, and consultant teams to inform best practices for engaging Hubbards community members.
- + Mobilize the project team and partners to promote and share engagement sessions, and inform community members about the process and timeline.



Figure 5: Aerial Photograph of Hubbards Cove

3.3 METHODS OF CONSULTATION

Based on the goals of the consultation for the Hubbards Community Plan, a range of tools was selected to facilitate the dialogue with the community. This section describes the consultation tools chosen, and details how they were applied in this context.

LAUNCH WEEK

The public dialogue was kickstarted by a week of concentrated communication through various channels. Using a social media campaign, the public was made aware of the Hubbards Community Plan Project and received daily updates with information materials. The campaign sparked interest in the project and gave an outlook on upcoming consultation activities.

SOCIAL PINPOINT

The 'Social Pinpoint' platform allowed community members to provide opinions, ideas, and feedback through a customized and interactive online map. The community was able to provide feedback using map markers of six different categories. The topics included commercial development, cycling infrastructure, pedestrian infrastructure, placemaking features, residential development and traffic management. Visitors of the

web site were able to interact with markers placed by other members of the public by indicating likes, dislikes, and open comments.

The interactive 'Social Pinpoint' tool was very effective in achieving a great response rate, including:

- + more than 2,500 visits to the site,
- + 877 unique users, and
- + 311 comments by 83 people.

These statistics represent a summary of activity in the timeframe between April 7th and July 1st 2021.

ONLINE GROUP DISCUSSIONS

Members of the community were invited to participate in several online discussions. Each of these sessions was widely advertised through mailing lists and social media. Participants were able to join the meetings through an online videoconferencing platform.

Meetings began with a presentation about the project background and an overview of sites and themes with relevance for the community.

After that, groups were split up into break-out rooms with one moderator for each virtual table. The small group sizes allowed residents to be more comfortable in voicing

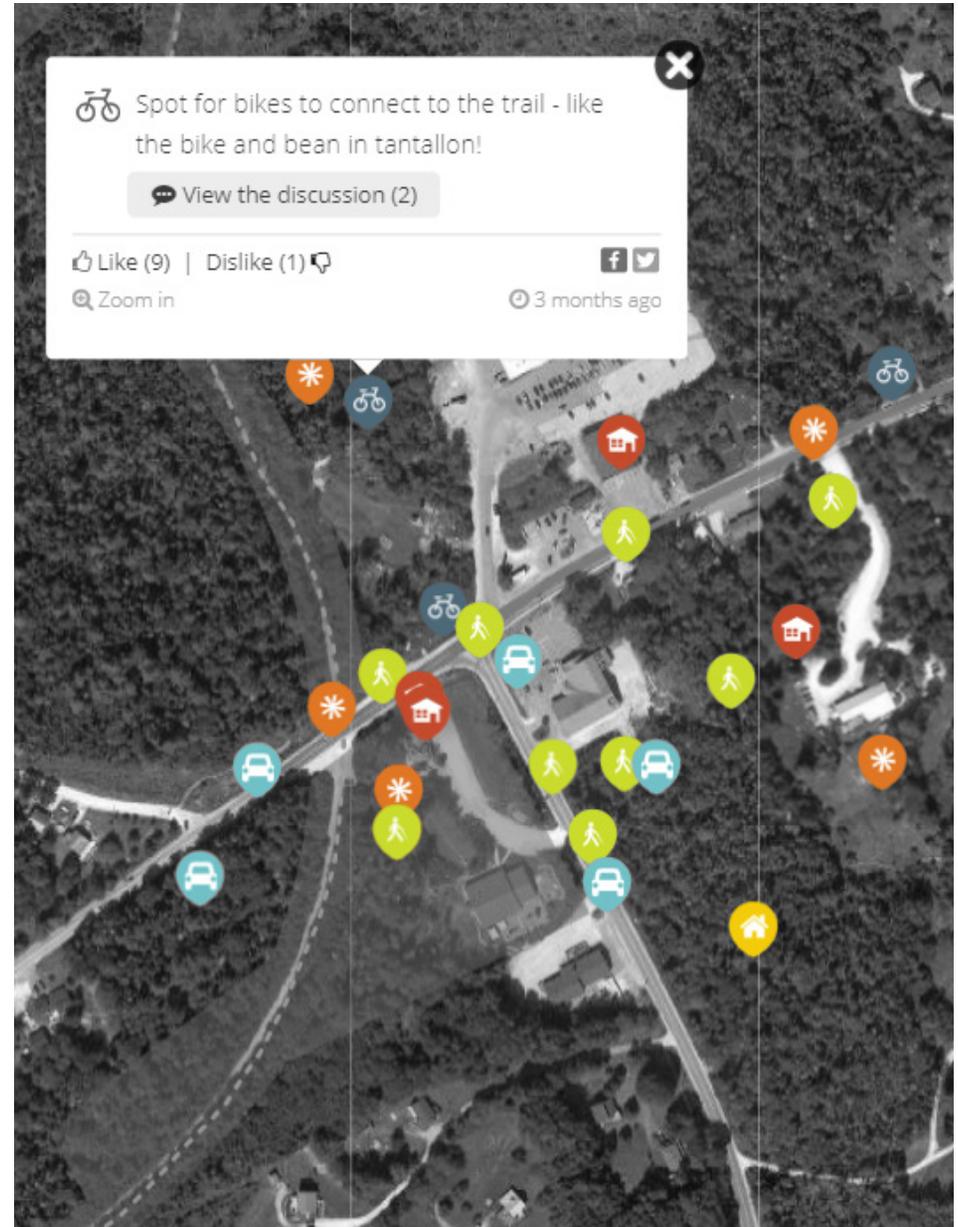


Figure 6: Snapshot of Social Pinpoint Map

their opinions, and facilitated lively discussions about improvement ideas for Hubbards. At the end of the meetings, all participants reconvened for a final recap and summary.

COLLABORATION COMMITTEE

The Hubbards Community Plan is an initiative of the Hubbards Streetscape Project, a local community group supported by the Aspotogan Heritage Trust. Since neither of these two organizations has any administrative authority over land use planning or road ownership, it was very important to include government authorities early in the process.

For this reason the Collaboration Committee was formed. The first meeting of this committee was held on May 7th and included participation from the Nova Scotia Department of Public Works, Halifax Regional Municipality, and the Municipality of the District of Chester.

All invited parties were able to exchange their views and expectations related to the Hubbards Community Plan. The Committee will be continued throughout the project and will serve as a platform to communicate the project results.

STAKEHOLDER INTERVIEWS AND WORKSHOP

The engagement effort included reaching out to groups previously identified as 'stakeholders' in the community planning process.

Stakeholders included organizations operating civic buildings, community associations, and owners of key lands in the community. They typically brought a different perspective to conversations, as they had gained insights into community needs and aspirations through their daily operations, and therefore had a broader view of the community context.

All selected stakeholders received an invitation to participate in a workshop on May 26th. The workshop format was similar to the residents' group discussions, with an introductory presentation for the whole group followed by smaller dialogues in break-out rooms. A lot of these conversations focused on land and infrastructure.

YOUTH AND SENIORS FOCUS

There were specific activities designed to engage both youth and seniors in the Hubbards community.

A youth focus group was held through a video conference in order to gain a perspective from that age demographic.

Consultation with a seniors focus group specifically for this project is forthcoming, and planned to be completed as part of the draft plan review process later this summer, now that public health restrictions are lifting.

INCLUSION INTERVIEWS

A series of one-on-one interviews were held to ensure that a wide range of street and public realm users' perspectives were heard and included in the engagement process.

These interviews were conducted with members of the community who would have unique experiences or challenges, or who were unlikely to participate in other engagement methods for a variety of reasons.

3.4 CONSULTATION RESULTS

SOCIAL PINPOINT (SPP) RESULTS

The interactive mapping and commenting exercise resulted in a wide array of community perspectives. Responses on the map can generally be divided into two groups:

- + responses relating to general themes; and,
- + responses relating to specific locations.

Both response categories are reviewed in the following sections.

SPP GENERAL THEMES

ROAD SAFETY

Throughout the social pinpoint map, participants noted numerous streets and intersections that they felt required traffic control measures to improve safety. There were also specific calls for infrastructure designed for vulnerable road users, including people walking and cycling.

A very large portion of comments centred around suggested solutions that could help overcome these challenges. Participants called for expanded trail networks, including new foot and bike paths to connect with existing trails, the beaches, and community amenities.

Bike lanes, sidewalk improvements, and the widening of shoulders were consistently mentioned as desired changes for roads in Hubbards, with many participants stressing how many blind spots currently exist along major roadways such as St. Margaret's Bay Road, Route 329, Fox Point Front Road, Shore Club Road and approaches to beaches.

Blind corners and blind crests were highlighted as areas where people feel especially unsafe walking and cycling. For example, participants drew attention to the limited visibility in both directions from the sailing club and Cannery Beach, at the Ten Beaches manufactured home community, and at the hill of Shore Club Road.

Many participants expressed concern about a lack of sidewalk infrastructure throughout Hubbards. When map markers related to this subject received comments, map users elaborated on the importance of paved shoulders on roads, and how sidewalks and bike lanes increase safety, comfort, and help to reduce the speed of traffic.

Participants discussed the need for road maintenance and proper shoulders along St. Margaret's Bay Road, and are worried that the new Vinegar Lake Road will increase traffic

on St. Margaret's Bay Road. There was also concern about the impacts of increased traffic on the school, the recreation centre, and local homes.

Participants noted how signage at Exit 6 feels busy and distracting to drivers and visitors, and that they should be tidied up to improve the first impression of the community. Commentators agreed, saying the signage should be cleaner and more uniform.

In addition to the examples referenced above, other suggestions include:

- + 'Through the Years' daycare use the Lighthouse Route often. Sidewalks and crosswalks

are necessary for the safety of children on this route.

- + Traffic slowing measures could help improve safety at the approach to Fox Point Front Road from St. Margaret's Bay Road.
- + A crosswalk is needed on St. Margaret's Bay Road at Shankel Road, in front of the Shatford Elementary School.
- + There is an unmarked intersection with a blind corner near the manufactured home park. Small children cross the road here to wait for their school bus on a narrow shoulder. Intersection infrastructure and signage is needed, and a speed limit reduction was called for.

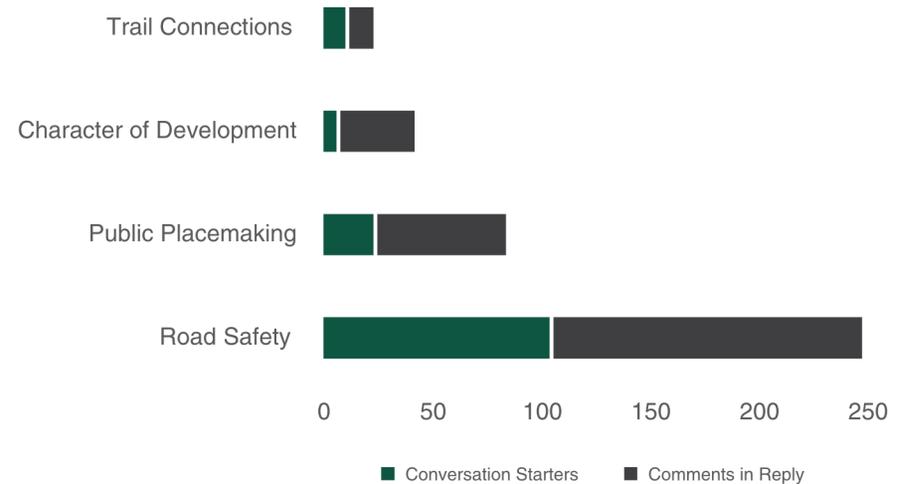


Figure 7: The cumulative number of SPP comments and replies to comments, grouped by theme.

PUBLIC PLACEMAKING

Along a stretch of St. Margaret's Bay Road, a participant suggested introducing more shops and cafes to emulate the feel of Mahone Bay. This was disliked by a majority of respondents, although it is not clear what exactly fueled these concerns. A similar map comment (also mentioning Mahone Bay) for lands of the Aspotogan Heritage Trust in proximity to the 'Barn' was very well received.

Participants feel that the public right of way along Conrads Road has the potential for a path connecting residences of the area to the elementary school, thus providing an alternative to walking along the street.

Participants suggested adding an official Visitor Information Centre to the Aspotogan Heritage Trust building, so that visitors can stop and seek information when coming through Hubbards. It was argued that this location would be equally serving motorists on the road and users of the Rum Runners Trail. It was mentioned that this is already an interesting location that could be used to promote local businesses and points of interest.

There was a mix of agreement and disagreement over creating

a larger parking area for the Rum Runners Trail, which could potentially increase the number of people walking and cycling on the trail and through Hubbards.

Participants are interested in seeing the vacant lot near the corner of Fox Point Front Road and St. Margaret's Bay Road either redeveloped or transformed into a place with outdoor seating or gazebo, artwork, and gardens.

Further placemaking improvements suggested by participants included:

- + Adding bike racks at beaches
- + Community outdoor BBQ & pizza oven near the Barn market
- + Implementing more 'Little Libraries'
- + Adding a small food concession at the beaches
- + Incorporating outdoor waste receptacles and waste collection throughout the community
- + Upgrades to the plaza near the Pharmasave

CHARACTER OF DEVELOPMENT

Throughout the social pinpoint, users stressed the importance of maintaining the culture and character of Hubbards when building and revitalizing commercial and residential

development. Participants discussed the importance of supporting and prioritizing new and existing local businesses over corporate chain stores in new developments. They touched on the local landscape, and how new developments and housing should be integrated into the landscape and maintain the so-called "community vibe".

Participants discussed having plans in place to prepare for the ongoing twinning of Highway 103, which may result in potential future interest in highway-oriented commercial development along that corridor, such as a drive-through restaurants and service stations.

Some people liked the idea of having a gas station that would have longer hours. Other participants worried that any discussion about bringing in chain stores would take away from the essence of what makes the community special, and that there should be a focus on local business rather than big business. Others believed that Hubbards shouldn't have a service stop along the highway, as they worried about garbage litter and how a service station would look.

On another note, there were also conversations about commercial development in general. While

maintaining the local character seems important to the map users, a large share of the comments called for new commercial features in Hubbards. The wish list included more cafes, shops, and a pub. On the matter of residential development, there were some comments about the need for affordable housing options. Some comments were critical of the idea to introduce multi-unit housing in Hubbards.

TRAIL CONNECTIONS

Many participants suggested extensions and connections of the Rum Runners Trail to other parts of the community, including:

- + A new connector trail at the eastern end of the community, linking the Rum Runners Trail with St. Margaret's Bay Road at the Hubbards Recreation Centre. A crosswalk would make it easier for people to access the trail and the recreation facilities.
- + Create an accessible multi-use trail from the Rum Runners Trail to Queensland Beach to encourage more people to bike rather than drive.

LOCATION SPECIFIC THEMES

HUBBARDS 'MAIN STREET'

There was a large amount of comments related to what can be described as the 'main street' in Hubbards, i.e. the stretch of St. Margaret's Bay Road going through the community.

Bike lanes are desired along St. Margaret's Bay Road in Hubbards, and participants would like the bike lanes to connect to the library, the elementary school, and the skate park and recreation centre. Parents commented that the skate park and recreation centre can only be reached by car, and that they would like their children to have safe bike lanes to get around the community.

Participants would also like to see a path across the government lands between the elementary school and the recreation centre, as well as a connector from there to the Rum Runners Trail. They would also like to see signage drawing attention to the dog park and trails.

Many comments expressed a desire for a cycling hub with bike racks and air pumps near the library and coffee shops so cyclists can stop and relax. Others would also like to see more outdoor seating, an outdoor

play area, and access to the trail in that location. This is an opportunity to increase the library's position as a central hub for the community.

Some participants noted that if bike lanes were built in the centre of Hubbards, they would love for them to continue along to Queensland Beach as the road is rough and narrow. Another participant commented that the road is so narrow that they would fare better putting in a paved shoulder instead of a bike lane.

SHANKEL ROAD

The old garage on the corner of St. Margaret's Bay Road and Shankel Road drew much attention and was highlighted as an area for improvement, though participants disagreed on whether the owners of that private property would be better off repurposing the garage as a local cafe, shop, or diner, while others preferred the site be cleared for landscaping and benches connected to the trail network.

Another participant mentioned that Shankel Road is one of the narrowest roads, and is often used as a shortcut when coming off of St. Margaret's Bay Road in Queensland to Conrads Road, turning onto Shankel Road and carrying onto Shore Club Road

or back onto St. Margaret's Bay Road. Drivers are said to exceed safe speeds, and it was noted that there is no real shoulder for pedestrians. Participants proposed a sidewalk or a multi-use path here.

SHORE CLUB ROAD

Participants would like to see a walking path along Shore Club Road as they find it very narrow, with little visibility due to the winding nature of the road. Participants called for lower speed limits, sidewalk improvements, and measures to increase visibility as the road is used by pedestrians

and cyclists heading to the Shore Club and Hubbards Beach, many of which are children and youth. A participant commented that they had been hit by cars while biking on that road. However, one participant also noted that compared to St. Margaret's Bay Road and Route 329, Shore Club Road is relatively safe and that limited resources for safety measures should be directed to other parts of the community first.

Shore Club Road does include a blind crest, which is perceived as particularly problematic. Participants stated that in the blind spot close to



Figure 8: Junction of St. Margarets Bay and Shankel Roads

the Tune Blue Inn, cars are unable to see pedestrians or cyclists going towards the Shore Club from the Anchorage. One suggestion was to incorporate a large convex mirror in the short term, to give pedestrians and drivers a line of sight around the corner until the issue is resolved.

The intersection of Shankel Road and Shore Club Road was highlighted as another place of concern as people are coming off Shankel Road, right onto the bridge without stopping or waiting for those coming off the bridge to come through. Suggestions were made for 3-way stop signs, larger signage, and modifications to the placement of signage.

HUBBARDS SAILING CLUB AREA

The Hubbards Sailing Club receives up to 70 sailors aged six to 16 through July and August, as well as sailors from the Through the Years Day Camp & Community Centre. Participants from the Through the Years Day Camp commented that they feel unsafe walking with children down the road. The main parking lot is across the road, and they have had many close calls. Suggestions included a pedestrian overpass, a speed limit reduction, and a sidewalk and paved shoulder.

HUBBARDS HARBOUR WALK

There were mixed feelings about a request to consider the model of Bar Harbor's Shore Path when developing pedestrian infrastructure plans along the Harbour Walk, looking to Bar Harbor for inspiration regarding accessibility, limited mobility, and pets. There was a mix of agreement and disagreement on this comment, with one person mentioning that large sections of such a 'Hubbards Harbour Walk' would be on private land and that residents should be consulted before changes are made.

QUEENSLAND BEACH

Participants called for ways to improve Conrads Road en route to Queensland Beach, around the tops of Conrads Branch Drive and Johnson Road. Community members do not feel safe there. Pedestrians also deem the stretch of St. Margaret's Bay Road from the community centre to Queensland Beach dangerous as they find the shoulder is not wide enough to walk on safely, and parents do not have room to push strollers. The hills and curves create limited sightlines, and community members found that quite dangerous. One participant called for the speed limit to be reduced to 50 km per hour.

At Queensland beach, participants hope to see:

- + Bike racks
- + Garbage cans and waste collection year-round
- + Upgrades to change rooms and toilets
- + A small food concession
- + A little park and space for outdoor activities
- + Kayak access points

HUBBARDS BEACH

Some participants noted how they used footpaths that private property owners have allowed them to use, and that they would like to see a sidewalk or connector to Queensland Beach near Conrads Branch Road and Johnson Road. This comment received considerable support from other participants.

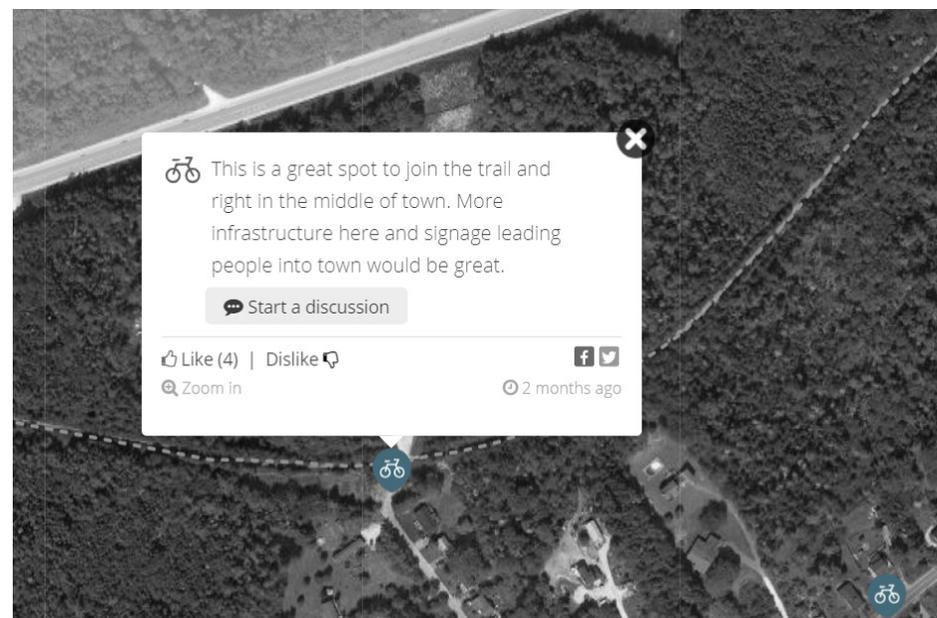


Figure 9: Example of Social Pinpoint Comments.

PUBLIC DISCUSSION RESULTS

Road safety emerged as a dominant theme in the discussions. Participants felt that there are various dangerous spots for pedestrians in the community, and that vehicle speeds are often beyond what is acceptable. However, there is a desire to get around the community on foot, and there is a call to design pedestrian connections with safety and vulnerable road users in mind.

Walking destinations (i.e. community facilities) are also perceived to need accessibility improvements,

which could create an overall better walking experience in Hubbards. Additional trail connectivity was discussed as a partial solution, with connections between the Rum Runners Trail and other parts of the community drawing the most attention. Private land ownership was discussed in the context of a future trail connecting Conrads Road with the Hubbards Recreation Centre.

Related to the desire of getting around the community is also a need to have a defined central community



Figure 10: Mobility patterns shown on a slide of introductory presentation to group discussions

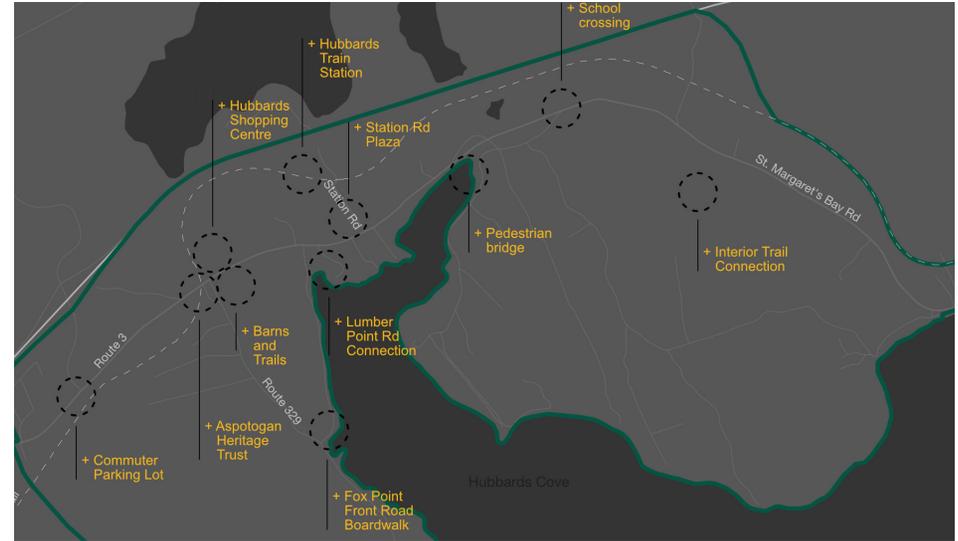


Figure 11: Opportunity sites shown on a slide of introductory presentation to group discussions

area, which residents noted as currently missing. Some discussion participants wished for more local shops and community facilities that improve the looks and experience in the community. Concerns were raised about the aesthetics and layout of the existing commercial development in the community, with a desire to see change but a lack of a clear path forward.

Discussions have shown that there is a desire for some form of 'village centre' at the intersections in proximity to the post office and library.

Related to community appearance, some points were made about

wayfinding. Several participants felt that signage feels 'cluttered' and inefficient in the community, especially in proximity to the Highway 103 Exit. Wayfinding improvements were generally suggested at junctions within the existing street network, so that navigating the community becomes easier and trail users are drawn into businesses.

Pedestrian signage is also seen as an option to improve accessibility for all ages, and to encourage active exploration of the community.

Coastal access was identified as a central pillar of the community identity, and participants characterized

Hubbards as a 'coastal walking community'. Opportunities identified for placemaking along the waterfront included a boardwalk along Fox Point Front Road to the Sailing Club.

On the point of overall community development, an intergenerational approach to designing programs and experiences came through as a priority during these discussions, with examples including arts and crafts and landscaping elements, as well as structures for rest and play. There was also discussion about the concept of aging in place, and the changes needed in Hubbards to facilitate that.

Participants recognized a lack of housing in the community, with few properties being available for purchase and rental options being essentially non-existent. Discussions revolved around affordable housing options, and those specifically geared to older adults.

Transitional care was proposed for some locations, with the added suggestions of a safe walking loop around any such development as a way to encourage physical activity among the aging population.

Environmentally sensitive conditions and privately owned land were each cited as barriers to development in certain cases. Freshwater drainage from Sawler Lake was identified as an issue in the vicinity of the Irving Station.

Temporary and seasonal fluctuations in parking demand was another challenge that emerged from these discussions, and a need to integrate a walkable community core into any central parking zone emerged as an opportunity to address that. Specifically, there was support for sidewalk network expansions on

St. Margaret's Bay Road and Shore Club Road, shelter structures at the current park-and-ride and at the former train station site, and for bicycle parking improvements at the shopping centre.

During the introductory presentations to group discussions, the project team was showing some locations identified as 'opportunity sites' with improvement proposals listed as bullet points (see Figure 11). The chart below summarizes how discussion participants responded to these suggestions in polls.

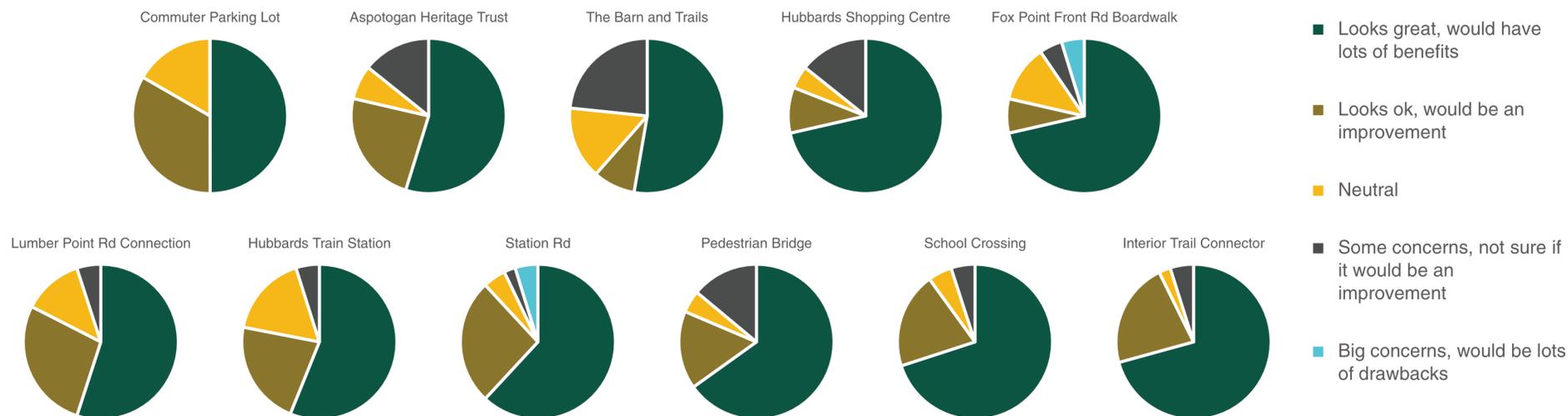


Figure 12: Zoom poll results from the public discussion about eleven different theoretical opportunity sites in Hubbards.

YOUTH FOCUS GROUP

In June, a group of twelve youth convened on a facilitated Zoom call to talk about their youth perspective on public spaces and mobility in Hubbards. Discussions touched on the ways they interact with their local environment and how they get around their community.

According to the focus group participants, Hubbards youth mostly get around their community on foot and by bike, with the occasional ride from parents depending on the trip distance or destination. Youth noted that walking and biking can be scary in some locations due to fast-moving traffic. Specific locations cited include near the Hubbards Sailing Club, along Conrad Road, and near Mr Bean's Market on St. Margaret's Bay Road.

Suggestions from the youth about how to address their traffic safety concerns included new sidewalks, bike lanes, and trails. They also suggested introducing button-activated flashing beacons at various crosswalk locations including at the bridge, the recreation centre, the intersection of Shore Club Road / Schwartz Road / Shankel Road, and the intersection of St. Margaret's Bay Road and Route 329.

Beautification and community amenities were also discussed, and the youth suggested there is a need for more bike parking at commercial destinations, and for more public garbage cans.

Other ideas included flower boxes and picnic tables throughout the community, a water slide, a community garden, and a playground area at the waterfront.

WHERE IN HUBBARDS DO YOU LIKE TO GO?

- + Skatepark
- + Hubbards Beach
- + The Barn
- + Rails to Trails
- + Mountain Biking Trails near Queensland Beach
- + Gallant's Fish & Chips
- + Cannery Beach
- + The Waterfront (swimming in the cove)
- + Fitzroy River
- + Mill Lake
- + Sailing Club



Figure 13: Hubbards Skate Park

STAKEHOLDER DISCUSSION RESULTS

These conversations largely revolved around the two themes of connections and public amenities. Participants pointed out that many of the commercial destinations in Hubbards are only accessible from St. Margaret's Bay Road, which generates significant traffic congestion at peak periods, creating potential for pedestrian and cyclist safety issues. In particular, the intersection of St. Margaret's Bay Road and Route 329 was noted as being especially problematic. Realigning the driveway to the shopping centre with that intersection was proposed as a way to improve traffic flow at this location.

The Rum Runners Trail was discussed as an important asset for the community, and participants suggested that new trail connections to key destinations along the trail would improve access to the trail, while also improving connectivity between other local destinations. It was noted that several community destinations, such as the community waterfront and the Hubbards Barn, are not easily seen from the street, and that wayfinding signage could be beneficial in these cases.

There was also interest among



Figure 14: Corner of St. Margarets Bay and Yacht Club Roads

participants in exploring ways to maximize the use of public spaces throughout the year, thereby establishing Hubbards as a year-round destination.

It was noted that many public amenities go unused for much of the year, and that the use tends to

fluctuate based on seasonality and depending on programming. Demand for both car and bicycle parking was also noted as an area of concern, and participants suggested that finding ways to connect the various parking facilities throughout the community might help address this.

Specific amenities identified as missing or needing improvement include sidewalks, picnic tables, public washrooms, bicycle parking, a bicycle repair kiosk at the library, and wayfinding signage throughout the community.

INCLUSION INTERVIEWS

Themes that emerged from these discussions were similar to those raised through other engagement methods: these themes included road safety, such as speed limits and the desire to see them lowered within the community's core, and the need for more active transportation infrastructure between key destinations.

Residents also discussed the need for more rental housing options and accessible housing for seniors, especially within the community's core where lower income or fixed income residents would not need a vehicle to access amenities.

Suggestions to address the lack of low-income housing included the addition of secondary suites or small buildings on existing

residential properties (as per the recent changes to HRM's planning policy) and adding another seniors complex to Hubbards which would meet accessibility requirements.

Multiple interviewees mentioned the importance of aging in place. Residents were keen to see underutilized spaces given more functionality for the community, such as the intersection of St. Margaret's

Bay Road with Fox Point Front Road as well as the Community Waterfront.

The latter was referenced as a key community hub and asset that is not being used to its fullest potential. Potential uses included additional seating/picnic areas and community concerts and events. Interviewees would also like to see the addition of a dairy bar or ice cream stand to the area for locals and visitors.

SUGGESTIONS FOR IMPROVEMENTS FROM INCLUSION INTERVIEWS

- + Lower speed limits
- + Additional speed mitigation tactics for hills/blind crests/turns
- + More vigilant trimming of vegetation around narrow roadways
- + Increase RCMP patrols to curb speeding
- + Sidewalks or widened, paved shoulders on major roads (like Hwy 3)
- + Preference to see "curbed" options for added safety
- + Crosswalks at key intersections
- + More connections from the rail trail into the community
- + Improvements to the intersection at the Shopping Centre
- + Rental housing options (especially for low income families and seniors)
- + Accessible housing options for seniors
- + More areas for people to picnic or eat takeout from local businesses
- + Need an ice cream stand
- + Updates to the Community Waterfront to increase form and function
- + Street beautification

3.5 SUMMING IT UP

SUMMARY OF KEY THEMES

A wide range of topics emerged from all of the engagement processes, but generally these topics can all be grouped into two primary themes and two secondary themes. The primary themes are 'road safety' and 'public placemaking', and the secondary themes are 'community character' and 'trail connections'.

The bulk of concerns voiced through various consultation formats related to the two primary themes, with the specific topics of pedestrian and cyclist safety

and community gathering places generating a high level of interest.

Key topics of interest relating to the secondary themes included support for small business, control of highway-oriented development, and trail connections between key community destinations.

SUMMARY OF IMPLEMENTABLE PROJECTS

All of the suggestions for implementation that arose through the public engagement effort can be grouped into three broad categories:

those that require the use of public land, those that require the use of private land, and those that intersect both public and private lands.

PROJECTS ON PUBLIC LAND

A number of suggested road safety projects came out of the engagement process, many of which would require either modifications to existing road infrastructure or new infrastructure within the public right-of-way. As roads are generally in public ownership, these projects fall under this heading.

In response to the resounding call for pedestrian and cyclist connections to

important destinations along St. Margaret's Bay Road, sidewalks are desired between Fox Point Front Road and Shatford Memorial Elementary School. Moreover, various types of pedestrian connections between the school and the Hubbards Recreation Centre are seen as desirable.

Improvement to the designs of existing crosswalks and installations of new crosswalks are a high priority for many residents in the community, and include various



Figure 15: St. Margaret's Bay Road in the Lunenburg County section of Hubbards features a short section of sidewalk.

pinchpoints along active transportation routes in Hubbards. The need to promote better walking and biking connectivity at busy road crossings is evident, especially as it relates to integrating the Rum Runners Trail into the community.

Calls for improvements to trail access centred around establishing connections to and between key community destinations. Specific examples included the Shatford Memorial Elementary School, the Hubbards Recreation Centre, Queensland Beach, and the Hubbards Barn Association. In some cases, such as between the Rum Runners Trail and the Aspotogan Heritage Trust, or between the Pharmasave and the Barns, trail connections already exist but could be improved. In other cases, such as between the Rum Runners Trail and the Hubbards Recreation Centre, a new trail connection would be required.

Visibility issues along the many winding routes in Hubbards prompted a call for sightline improvements at specific locations on Route 329, Shore Club Road, Shankel Road, and Fox Point Front Road.

Signage was flagged as both something that Hubbards could use less of but also something that, if

implemented carefully, could help with orientation for both visitors and residents alike. Reducing the visual clutter at the Highway 103 exit was seen as a way to improve the community gateway condition by many, and new wayfinding signage at key road intersections as well as a new pedestrian-oriented signage program for Hubbards was suggested as a way to improve the walking and biking experience in the community.

Specific placemaking elements are also suggested for a number of other sites throughout Hubbards. This includes an outdoor pizza oven and concession stands for food vendors to create more momentum around central activity hubs in the community.

Infrastructure improvements are desired for several locations along the Rum Runners Trail, including a sheltered seating hut and bike repair stations at both the park-and-ride and at the site of the former Hubbards train station.



Figure 16: A stand-alone sign on St. Margarets Bay Road

PROJECTS ON PRIVATE LAND

A number of potential new features or community improvements were discussed that would require the use of private land, as well as permission from and cooperation with local landowners.

In most cases these suggestions would not involve changes to the land use, but rather would involve the introduction of street furniture such as little community libraries, bike parking racks, waste receptacles, and outdoor seating.

In several cases, frequently mentioned trail connections would have to traverse private land. This includes the restoration of an old trail connection between Conrads Road and the Recreation Centre, as well as a conceptual waterfront trail dubbed the 'Hubbards Harbour Walk' by consultation participants.

Housing was a topic that generated a lot of discussion through the engagement process. There were calls for affordable housing in Hubbards, specifically rental accommodation and transitional care facilities for older adults. A project

of that nature—whether situated on private or public lands—would need to be guided and supported by appropriate development policies.

PUBLIC-PRIVATE PROJECTS

Placemaking emerged as such an important element of consultation results that it requires particular attention. Landscaped plazas and streetscape improvements were suggested for a number of prominent sites along St. Margaret's Bay Road, including in the vicinity of the Irving Station and the Pharmasave, and

in the vicinity of the Post Office. The Post Office location was cited as a location with great potential for serving as a central community hub.

All of these opportunities for small parks and plazas straddle the boundary between St. Margaret's Bay Road and adjoining properties, and would involve both public and private lands. Placemaking opportunities should be conceived as an extension of the road realm in these cases, and thoughtful conversations between the road owning authorities and private land owners will be necessary for a successful implementation.



Figure 17: Example of temporary plaza on King Street in Lunenburg, NS.

MOVING FORWARD

Residents expressed that they would like a centre that feels like a village, rather than a larger town. Public engagement suggests the area around the library and near the waterfront should be the primary centre for place making investment. Stakeholders also desire a safe, attractive, vibrant village main street connecting the Shatford Library to the grocery store, providing a 900 metre walkable stretch between some of the community's primary commercial destinations. They express it is important to invest strategically along the main street to tie its sections together and to create a well-connected street that feels like a single whole.





4. ENGAGEMENT ON DRAFT PLAN

4.1 PURPOSE AND ACTIVITIES

In March 2022, a draft version of the Hubbards Community Plan was released to the public. The Plan was presented to the community on March 30th during an online information session. A digital version of the Plan was made available to the community on the project website and a printed version was available at the community library.

The project team also used an online interactive map and community workbook to garner feedback from residents and other interested stakeholders in the spring of 2022. The Hubbards Streetscape Project and UPLAND also organized a stand at the Hubbards Barn Farmers Market to create awareness about the proposed plan and to draw additional feedback.

Several changes were made to the plan as a result of the public feedback. This report section summarizes the findings from the public engagement on the Draft Community Plan, and explains how feedback was integrated into this final version of the plan.



Figure 18: Hubbards Community Plan stand at the Farmers' Market organized by 'The Barn'.

4.2 FEEDBACK ON COMMUNITY STRUCTURE

The Draft Community Plan proposed a community structure for the future of Hubbards. The community structure is intended to inform future updates to zoning in the community, and it consists of eight designations—with each designation proposing different policies for future growth in the community.

Generally, there was support among respondents for the community structure recommendations of the draft Community Plan. Respondents felt the proposed community structure would promote future development patterns that are desirable in the community.

Respondents also left specific suggestions regarding the proposed community structure, including:

- + The Natural Space Designation on the eastern side of the project area could be expanded to include a portion of the lands between Baden Road and Conrads Road to support trail use and access to nature.

- + The area between the Barn and the Fox Point Front Road/ Highway 329 intersection could be targeted for medium density and multi-unit development that is connected to the Barn with trails.
- + Additional considerations for the Trailside Residential Designation so as to keep the “natural” feel of the trail area and avoid over developing and changing the feel and atmosphere of the trail.

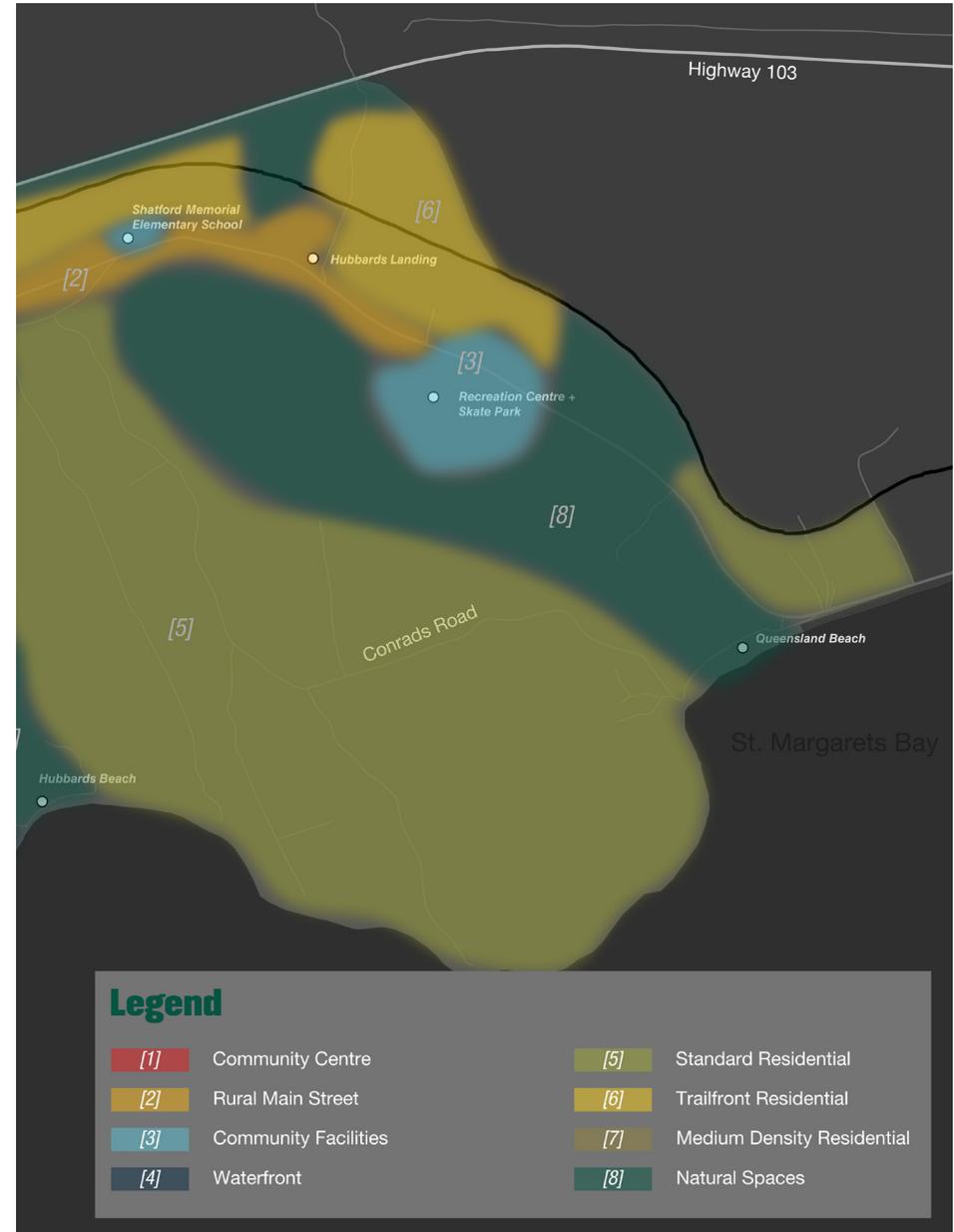


Figure 19: Part of Community Structure Map

4.3 FEEDBACK ON OUTLINED ACTIVE TRANSPORTATION NEEDS

A significant focus of the Hubbards Community Plan is to improve mobility, especially for active transportation users (i.e., walking, cycling, rolling, etc.). The draft Plan included proposed changes to the design of streets, lot accesses, and active transportation infrastructure to promote a safe and comfortable environment for pedestrians. For the purpose of this section, the feedback from residents and community members has been broken down to specific locations within the project area.

ST. MARGARET'S BAY ROAD

Respondents to the draft Hubbards Community Plan were generally supportive of the proposed changes along St. Margaret's Bay Road. Many respondents were supportive of creating a multi-use path and extending the sidewalk along St. Margaret's Bay Road; however, one respondent noted that there could be some redundancy with implementing both types of infrastructure (sidewalk and multi-use path). Respondents also supported the proposed intersection realignments and addition of crosswalks along St. Margaret's Bay Road.

Specific feedback related to the proposed changes along St. Margaret's Bay Road included:

- + There was a high degree of support for the realignment of the intersection at the shopping mall.
- + One respondent suggested including dedicated turning lanes at the shopping mall.
- + There was some concern about infrastructure changes including pedestrian bump outs, medians, and pedestrian areas regarding their cost, maintenance, and how they could interfere with existing operations like snow ploughing.
- + Some respondents suggested there is a need for a crosswalk at Yacht Club Road to access the library.
- + Respondents like the opportunities for placemaking but there was some concern about food trucks, and questions whether such vending activity could impact the quality of life in Hubbards.
- + A group of respondents noted that the multi-use trail should continue to Queensland Beach to provide access to this asset and to reduce

parking pressures at the beach.

- + There was general support for the changes around the elementary school that make the areas safer for children.

SHORE CLUB ROAD

The primary changes for Shore Club Road included creating formal links from St. Margaret's Bay Road with a bridge across the Hubbards River and a series of sidewalks and a boardwalk along Shore Club Road. Many respondents were very supportive of these changes, especially the addition of the boardwalk in Hubbards; however, one respondent expressed concern for the overall cost, maintenance, and access to private land that would be required to create the boardwalk.

SAILING CLUB

The intersection at the Hubbards Sailing Club (Fox Point Front Road and Highway 329) is the subject of a feasibility study by the Municipality of the District of Chester to reconfigure the existing intersection.

Respondents were supportive of

the feasibility study and excited by the prospect of changes to the intersection. Respondents were also supportive of the proposed medians and crosswalks at the Sailing Club to help cross this busy street.

4.4 FEEDBACK ON OUTLINED 'SIX BIG MOVES'

The draft Community Plan included 'Six Big Moves' to reshape the form and function of Hubbards. This section reports how respondents felt about each of the 'Six Big Moves':

HUBBARDS LOOP

The Hubbards Loop creates a loop around the core of the community with the addition of a new multi-use pathway along the northern side of St. Margaret's Bay Road. Among respondents, there was a high degree of support for the creation of the Hubbards Loop to help facilitate movement around the community. One respondent noted, however, that few people are using the existing trail, and developing the Hubbards Loop would not necessarily be needed because few people would use it.

SIDEWALK EXTENSIONS

There are a series of proposed sidewalk extensions as part of the draft Plan, including:

- + on Highway 329 past the Sailing Club to Cannery Beach;
- + along the southern side of St. Margaret's Bay Road to the waterfront; and

- + to connect St. Margaret's Bay Road with Shore Club Road.

Similarly to the Hubbards Loop, respondents generally supported the proposed sidewalk extensions in Hubbards, especially as they would help to promote safe movement through the community. There were two concerns expressed by respondents regarding the Sidewalk Extensions:

- + that there may not be a need for both a multi-use pathway and a sidewalk on St. Margaret's Bay Road; and
- + there were questions if sidewalk extensions could potentially impact private properties in the area.

COVE CONNECTIONS

The Cove Connection recommendations include the development of a boardwalk along Shore Club Road and improvements along Fox Point Front Road. Respondents to the draft Community Plan were supportive of the recommendations to improve active transportation in these areas, but some respondents expressed concern with the overall cost of the boardwalk and its maintenance and repair, especially concerning

coastal storms that could bring large waves to the area.

One respondent suggested using materials other than wood to build the boardwalk to ensure its long-term use and sustainability. One respondent wanted to ensure that the design of the boardwalk considers the needs of local wildlife and does not interfere with ecosystem functions.

PLACEMAKING

The Draft Community Plan identifies three areas for streetscaping and public space improvements:

- + the entrance to the Community Waterfront;
- + a central public square-like space by the Post Office; and
- + opportunities to continue to develop the space around the Aspotogan Heritage Trust.

Respondents were in support of the proposed streetscaping and public space improvements in Hubbards, especially efforts to improve the entrance to the Community Waterfront. One respondent noted, however, that food trucks could create disturbances and that a playground and larger park area would be better suited for this area. Another respondent identified

a property across the street from the former funeral home as another potential site for placemaking.

DEVELOPMENT POLICIES

The development policies of the ‘Six Big Moves’ are intended to implement the principles of the community structure. Each community structure designation—of which there are eight—had a series of proposed polices and development rules established in the draft Community Plan. Generally, there was strong support from respondents for the development policies of the draft Plan, with many respondents indicating they ‘strongly supported’ them.

Other feedback left by respondents regarding the development policies included:

- + One respondent was concerned the policies of the Community Core designation being too restrictive, and that the creation of a streetwall in Hubbards (i.e. stretches of St. Margaret’s Bay Road with a continuous row of buildings close to the road) is not in keeping with the character of the community.
- + One respondent was concerned with the policies for the Rural Main Street designation—specifically, they were concerned the policies and rules were overly restrictive for a rural community like Hubbards.

- + One respondent commented that light pollution is a concern in the community.
- + One respondent supported increased flexibility for multi-unit development in the Residential designation.
- + Several respondents suggested that protecting natural spaces and improving access to them are important for the future of Hubbards.
- + One respondent was concerned with promoting too much growth in Hubbards as the community is seen an alternative to living in the suburbs or within a busy urban area.

INTERIM MEASURES

The long-term changes to Hubbards within the draft Plan are accompanied by interim measures that could be implemented immediately by the community. The Plan highlights temporary bump-outs at crosswalks and intersections, painted road murals, and interactive elements (e.g., sitting areas, free libraries, etc.) as potential options.

Generally, respondents were very supportive of the implementation of interim measures, with one respondent suggesting facade improvements to existing commercial building would also benefit the community. However, although there was support for this recommendation, some respondents were concerned with the potential maintenance, cost, and practicality of some of the suggestions (e.g. bump-outs) could create challenges in the winter for snow ploughs while seating areas near St. Margaret’s Bay Road may be undesirable locations because of the traffic on the road.

4.5 HOW THE FEEDBACK WAS INTEGRATED

APPROACH

Feedback on the Draft Community Plan showed a clear support for the Plan and its proposed measures and policies. At the same time, there were some outlying opinions and critical comments received from the public, as documented on the previous pages of this report.

A balance needs to be struck between maintaining the core values of the Plan, which are based on the initial two rounds of engagement, and the comments received on the draft version of the Plan.

The project team met with the Hubbards Streetscape Project Volunteer Committee to evaluate the feedback and to integrate the comments in this final version of the report.

The following factors were given consideration when integrating the public responses into the document:

- + initial rounds of engagement and principles;
- + majority opinions;
- + practicability and feasibility of implementing certain suggestions;
- + constructiveness of the comments.

Due to the largely positive feedback received on the Draft Hubbards Community Plan, far-reaching modifications to the structure or rationale of the document did not appear needed or justified.

However, there were several elements of critique that were found to be appropriate and to improve the quality of the plan when addressed. These adjustments to the draft version of the plan are summarized in the following paragraphs.

DEVELOPMENT IN 'TRAILFRONT RESIDENTIAL' DESIGNATION

Several respondents to the public engagement have noted concerns about promoting development in those areas and changing the natural character of the trail surroundings.

It is not clear whether these respondents are aware of the fact that many lands on the trail are held in private ownership (some of them by developers), and can get developed anytime due to supportive zoning. Whether these lands will get developed is therefore largely outside of the control of municipalities and is more of a question of 'when' than 'if'. The Trailfront Residential Designation in this Plan is therefore not intended to promote development activity



Figure 20: Queensland Beach

around the trail, but rather to make sure that appropriate rules are in place **when** development does occur.

Nevertheless, the comments indicated that the original vision for 'Trailfront Residential' development may have entailed an overly urban character, and this has been considered through adaptations to the community designation and proposed development policies for those areas. As a result of the public feedback, this final version of the plan features a stronger focus on landscaping and tree retention for development abutting the trail, to ensure that the visual setting for trail users remains largely intact.

ACCESS TO QUEENSLAND BEACH AND RECREATION CENTRE

With regard to the comments received on extensions of active transportation infrastructure along St. Margaret's Bay Road, several factors needed to be considered.

On the one hand, it is recognized that both Queensland Beach and the Recreation Centre are key assets of the community and deserve proper access. On the other hand, the suggested eastern extension of a multi-use path past the JD Shatford Elementary School does not appear

to be an appropriate use of resources. The creation of sidewalks or pathways along St. Margaret's Bay Road would inevitably require large investments for removal of storm water ditches and the creation of engineered storm water management techniques. To perform such infrastructure works on the more than two kilometres of road from the school to the beach does not seem well justified, since there are very few residences or businesses along that stretch of the road which would benefit from such an upgrade.

At the same time, this portion of the road runs in close proximity to the Rum Runners Trail. Rather than duplicating infrastructure, there is an opportunity to use the existing trail when improving connectivity to community assets in the Queensland area.

Two additional cross-connections from the Rum Runners Trail were therefore added to this final version of the plan:

- + at the Hooked Rug Museum, a newly created provincial right-of-way can be used to connect to the trail, a crosswalk installation proposal between the museum and the Recreation Centre has been added to tie in another important piece of social infrastructure

- + a provincial Crown Property was identified as land that could potentially connect Queensland Beach to the trail, and the site has also been identified as another potential crosswalk location.

QUESTIONS ON THE SCOPE OF PROPOSED MEASURES

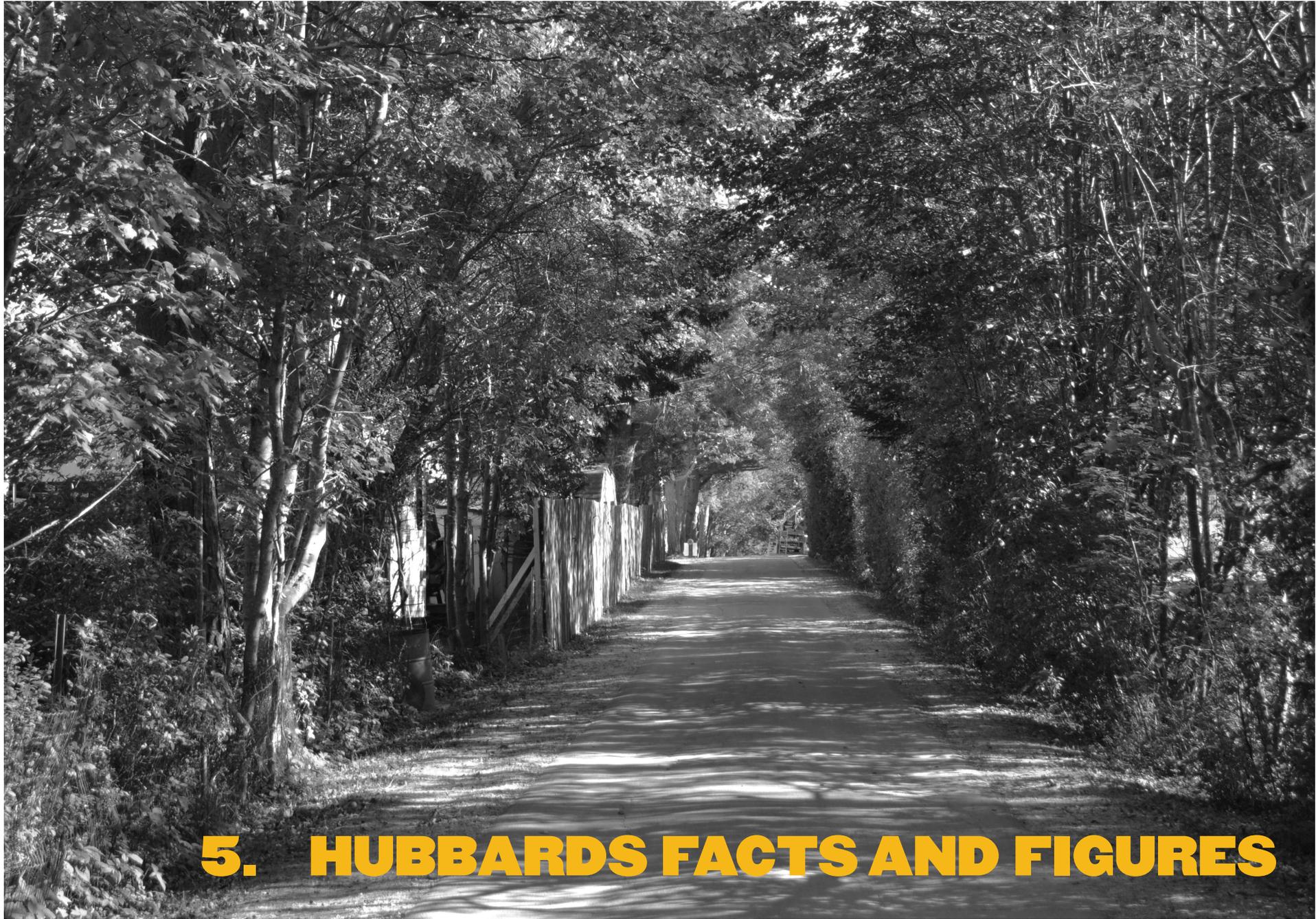
Comments relating to redundancy of proposed infrastructure or questioning the necessity of certain proposals were accommodated in the implementation section of this document. While active transportation infrastructure may not be immediately necessary on both sides of St. Margaret's Bay Road, the project team still recommends to target such an investment in the long-term. However, provision of a multi-use path on parts of St. Margaret's Bay Road which feature existing or proposed sidewalk was placed in Phase 4 of the project. Similarly, a sidewalk on the southern side of St. Margaret's Bay Road east of the Community Waterfront Park and a dedicated pedestrian bridge over Hubbards River were allocated in Phase 5 and labelled as 'optional'.



PART II

Community Analysis





5. HUBBARDS FACTS AND FIGURES

5.1 PROJECT AREA

The Community Plan covers an area which extends beyond the boundaries of the community. Hubbards is more than the buildings or roads within its jurisdictional boundary; it includes the people, institutions, businesses, and relationships that have grown in the area. You do not need to live in Hubbards to be a part of the community.

On the map below, the project area is shown and appears as a horseshoe hugging Hubbards Cove - a prominent inlet of the Atlantic Ocean within St. Margaret's Bay.

There are approximately 8.6 km² of land within the chosen project boundary. The western limit of the project area extends beyond Exit 6 and includes parts of Simms Settlement, while the eastern boundary roughly follows St. Margaret's Bay Road, extending just beyond the Queensland Lake and Queensland Beach.

The northern and southern boundaries for the project area are represented by tangible, distinct, borders. Highway 103 represents the northern boundary for the project area and the southern extent of the project area is defined by the Atlantic Coast.

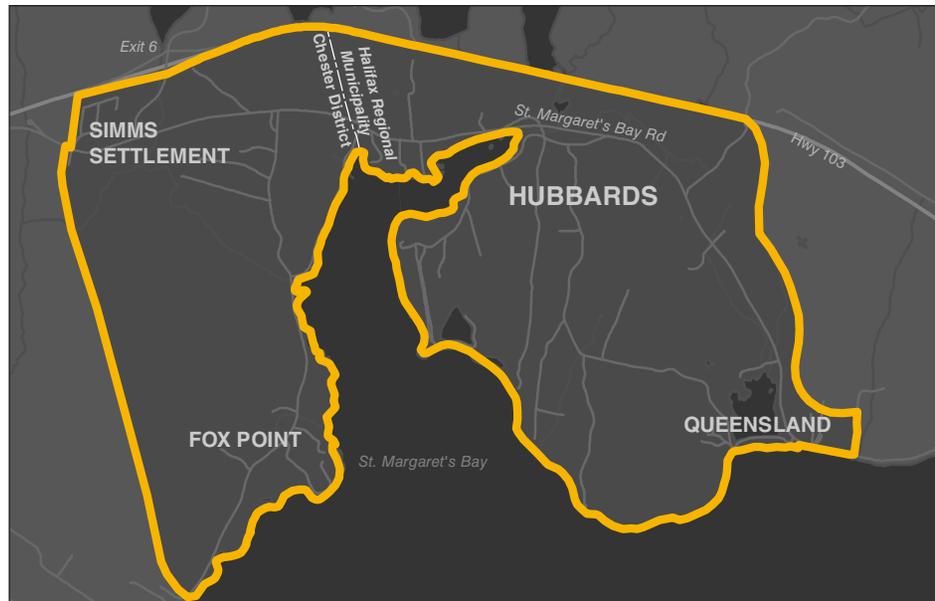


Figure 21: Community Plan Project Boundary

5.2 POPULATION AND DWELLINGS

Based on the 2021 Census, the project area has a population of approximately 1,187. This number is established through a combination of Census data and manual dwelling counts.

Geographic boundaries provided by Statistics Canada are only partially useful for Hubbards. Within the Census Dissemination Blocks shown in the blue outline below, Hubbards counts 796 residents in 490 dwellings.

120 of these dwellings are only seasonally occupied. This means that in the 370 dwellings with year-round

populations, the average household size equates to 2.2 persons that regularly occupy these dwellings.

Census Dissemination Blocks outside of the blue boundary overlap with neighbouring rural area and could not be utilized for this analysis. Instead, a manual dwelling count was applied to high-resolution aerial imagery, and resulted in 131 counted dwellings in Lunenburg County (west) and 47 dwellings in Halifax County (east). Using the population to dwelling ratio from central Hubbards, this adds an estimated population of 391 to the 796 provided by Statistics Canada.

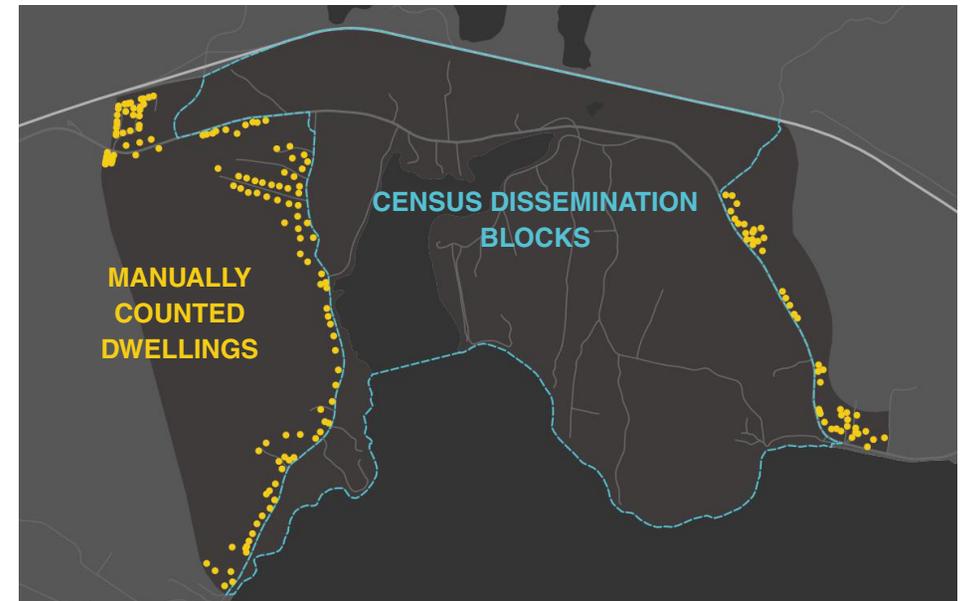


Figure 22: Dwelling Counts

5.3 ROADS IN HUBBARDS

The study area is connected to the neighbouring regions through a series of highways. Highway 103, which connects communities along the South Shore from greater Halifax to Yarmouth, runs parallel to St. Margaret's Bay Road (Trunk 3). The latter bisects the community along an east-west line.

There are also many collector and local roads within the project area, such as Route 329 and Conrads Road, that stitch the residential and commercial areas of Hubbards together.

The majority of roads the project area are owned and operated by the Province of Nova Scotia. The Department of Public Works is responsible for both Highway 103 and St. Margaret's Bay Road, in addition to many of the collector and local roads in the project area. However, sidewalks on these provincial roads are owned and maintained by the municipalities.

Private roads, which are most often owned by the residents that live along them, make up a significant portion of the roads within the project area.



Figure 23: Road hierarchy in Hubbards



Figure 24: Road ownership in Hubbards

5.4 ACTIVE TRANSPORTATION

Hubbards has a collection of formal and informal active transportation options for residents and visitors to use to move throughout the community.

The Rum Runners Trail (named after prohibition-era bootleggers known as 'Rum Runners') is a flat, multi-use trail that once served as the local rail route for train service along the South Shore. This trail runs from Halifax to the Town of Lunenburg and crosses St. Margaret's Bay Road within Hubbards; however, there is no formal crossing for the trail.

The trail can be accessed year-round, including for walking and snowshoeing use in the winter months.

The trail is owned by the Province, but its maintenance is delivered by the St. Margaret's Bay Area Rails to Trails Association, a community group which operates with support by the Halifax Regional Municipality.

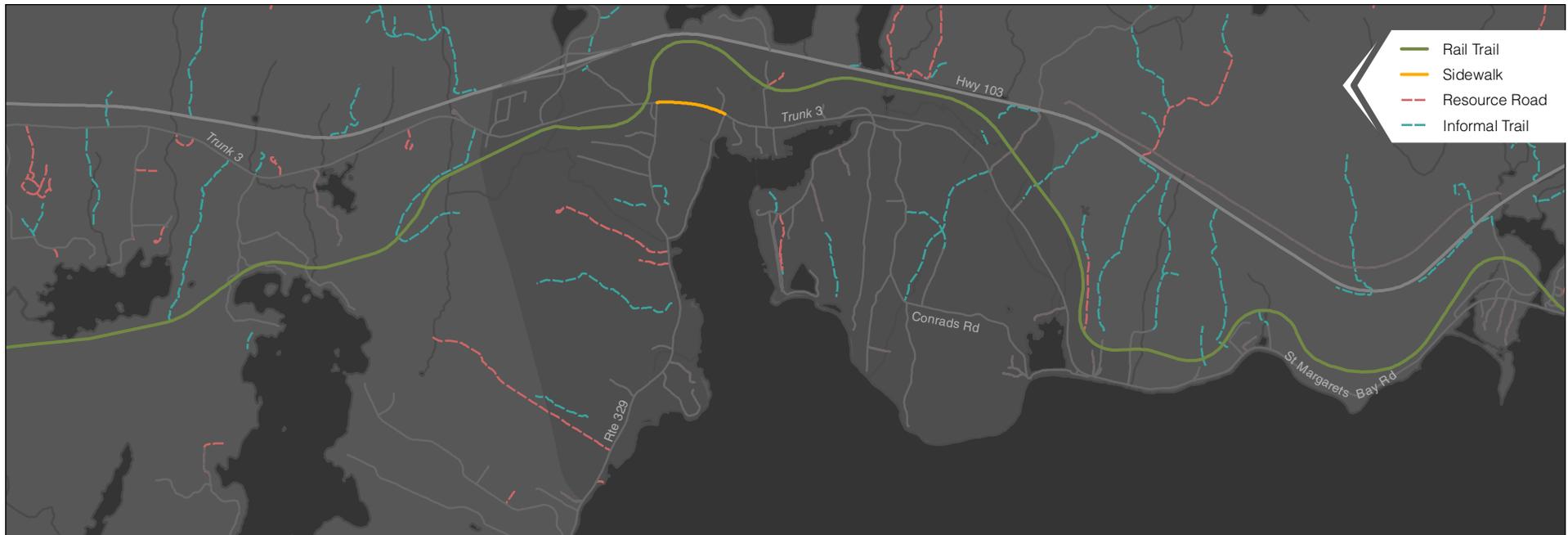
Approximately 100 metres from where the Rum Runners Trail crosses St. Margaret's Bay Road is the lone sidewalk in Hubbards. The sidewalk is located on the south side of St. Margaret's Bay Road

and is approximately 500 metres in length. The sidewalk is owned and maintained by the Municipality of the District of Chester.

Not all active transportation infrastructure and routes in Hubbards are formal. They can also include informal routes and infrastructure which residents use to get to places they live, work, and play. There are a number of such informal trail routes that weave throughout the project area, branching off existing roads or trails.

A Rural Active Transportation Framework is currently under preparation by the Halifax Regional Municipality. The draft document identifies Hubbards as a 'rural centre with existing main streets'.

Going forward, the framework will recommend formalized funding and prioritization related to construction of new sidewalks. The final version of this document is expected to go to Council in spring of 2022.



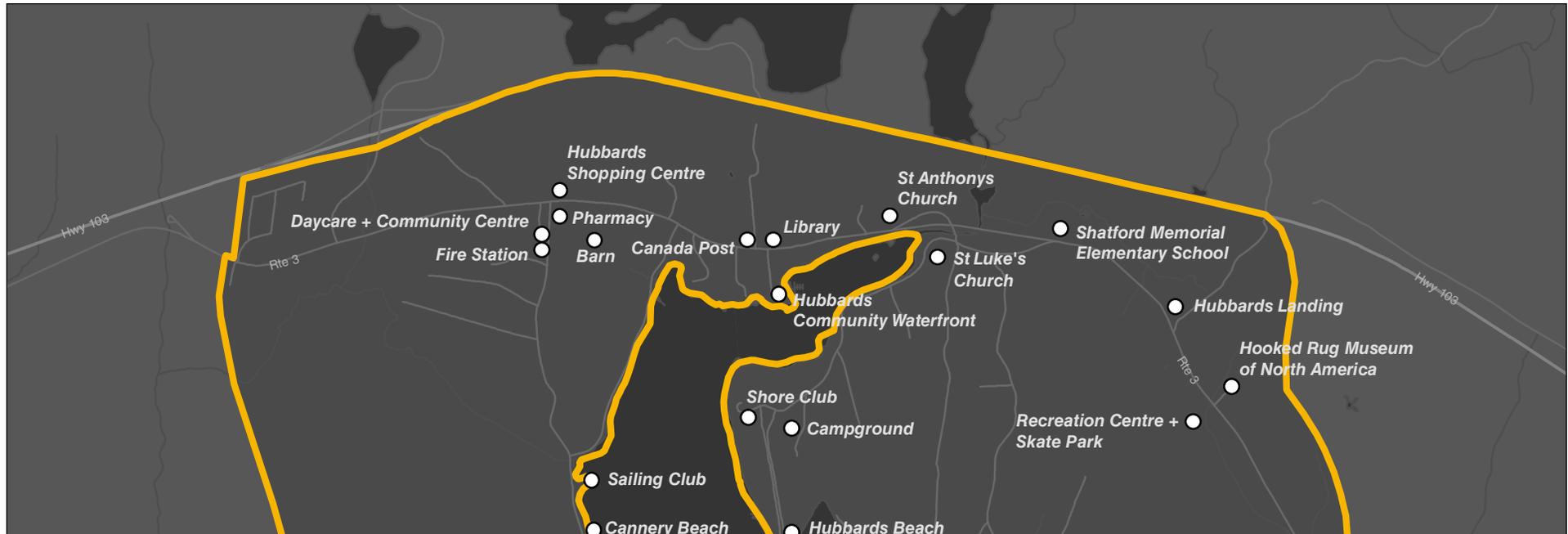
5.5 COMMUNITY FEATURES

At the heart of Hubbards are the many businesses, community organizations, and institutions that are foundational to the community. Community features in Hubbards are distributed linearly along St. Margaret's Bay Road, which serves as the primary conduit through the area.

In the westernmost part of the project area is a cluster of community features including the Community Daycare and Community Centre, Fire Station, Shopping Centre and pharmacy, which bookends the commercial and institutional core of Hubbards.

At the opposite end of the project area, the Hubbards Recreation Centre and Hooked Rug Museum make up the easternmost limits of the commercial area in Hubbards.

Within the area described above are a series of other features including the local elementary school, church and the local post office. These facilities are scattered along St. Margaret's Bay Road and contribute to the lively and active nature of Hubbards.



5.6 LAND USES IN COMMUNITY

Land uses in the project area are generally subject to relatively strict separation between residential, commercial, institutional and rural resource categories.

In the whole community there are only two mixed-use properties breaking with that pattern: both the Cove FM building at the bottom of Station Road and the Hubbards Landing complex feature both residential and commercial units. Apart from these two exceptions, all other properties accommodate only one category of land uses.

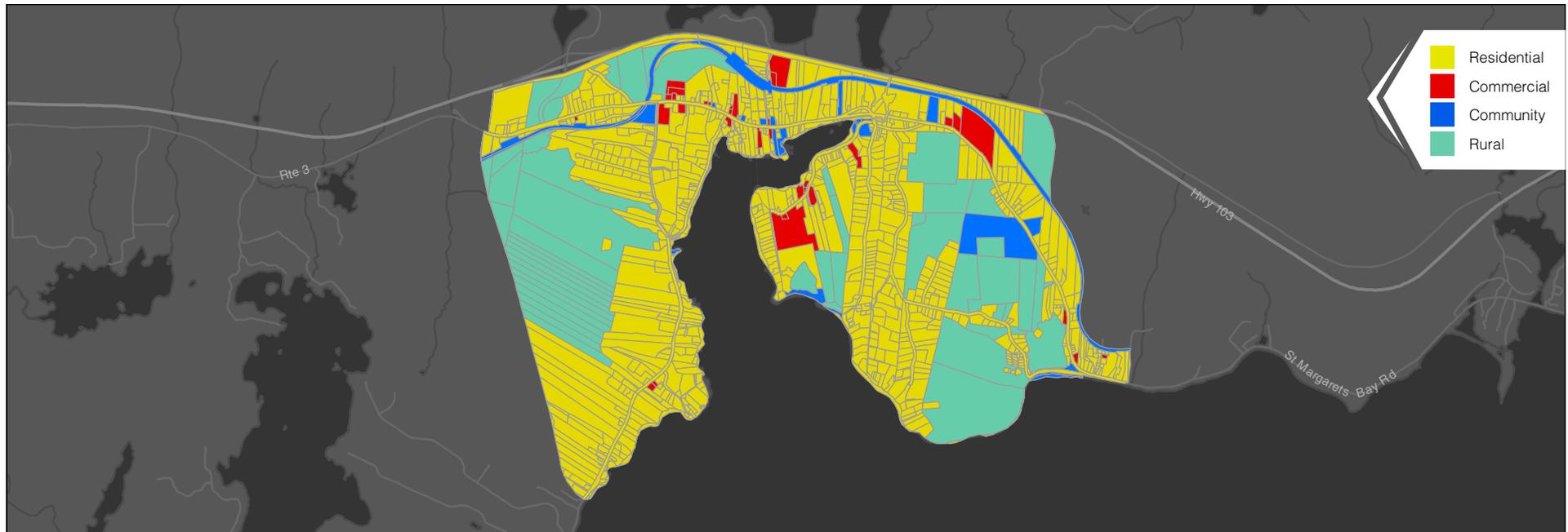
Residential properties are the most dominant type of land use in Hubbards. Especially properties abutting Conrads Road, Conrads Branch Road and Route 329 belong into this category.

Wooded lots, wetlands and other types of undeveloped, vacant property constitute rural resource uses in the map below. In most cases, however, there is no more active forestry practiced on these lots.

Commercial and community land uses are generally distributed throughout Hubbards, but tend to concentrate

around the core of the community. Exceptions exist both at the eastern fringe of the community and the eastern shore of Hubbards Cove. Most commercial properties are fully surrounded by residential properties.

The Rum Runners Trail stands out as an extraordinary property in public ownership, and spans the entire community from east to west. Otherwise, the largest community facility is the recreational hub on the eastern fringe of the community, featuring a skate park and baseball field among other amenities.



5.7 MOVEMENT PATTERNS - ORIGIN DESTINATION MAPPING

The composition of land uses in Hubbards has implications on residents' movement patterns through the community. Since commercial and residential spaces are rarely in the same building, some pockets of commercial and civic activity have developed. These are alternating with residential areas situated between these pockets.

For instance, the area around the Hubbards Shopping Centre is a cluster of commercial buildings. Other community features such as the Recreation Centre, Sailing

Club or the Hubbards Landing Mall are also separated from denser residential areas. Stretches of land with predominantly residential buildings are nestled between these areas, and showing as teal-dashed bubbles on the map below.

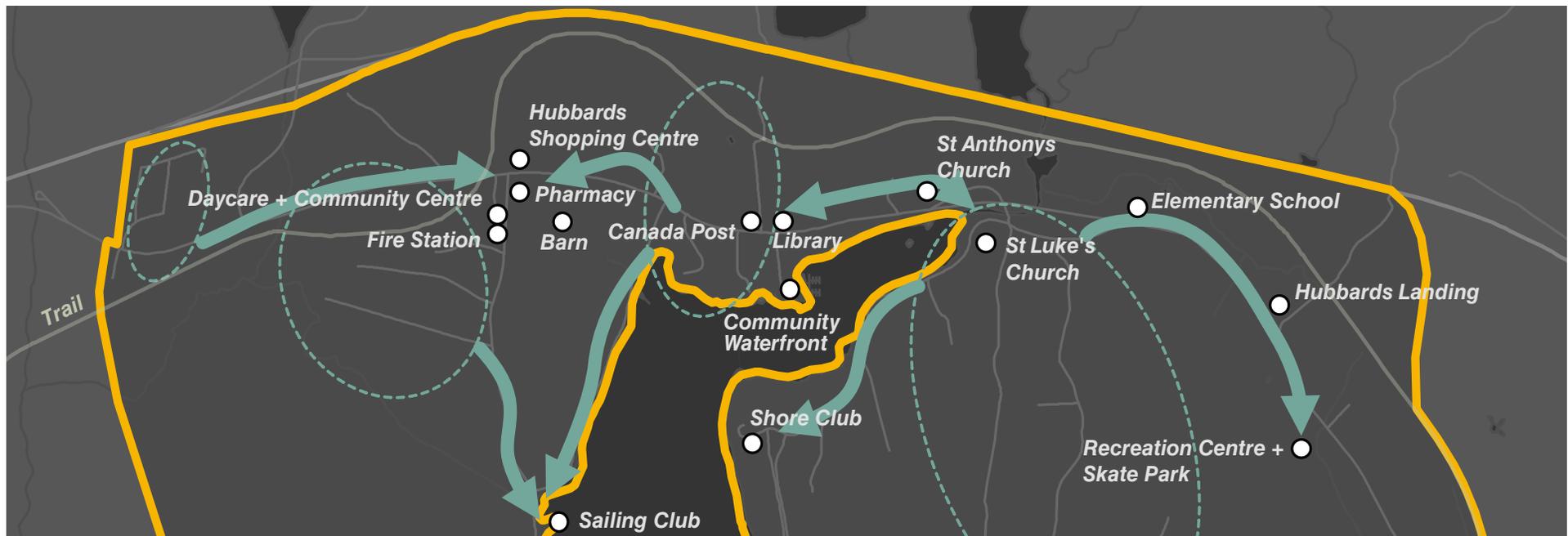
As shown on the map, a division between activity centres and residential areas becomes apparent. One major exception to this rule is the very centre of the community. Local destinations such as the waterfront, library, post office and Hubbards Cove Cafe co-exist in

close proximity with many homes in that vicinity. This area is distinct from the rest, because the historic village core evolved around the former railway station and followed a different development pattern than later parts of the community, which were built around the needs of car drivers.

In any case, this separation of land uses creates a lot of movement between origins and destinations on the map, as symbolized by arrows. These are the most logical connections that many residents will need to bridge on a daily basis,

and require infrastructure which can support these movements without reliance on cars.

In most instances, barriers such as St. Margaret's Bay Road need to be overcome for these movements. Infrastructure supporting active transportation is entirely absent in most cases.



5.8 ELEVATIONS AND FLOOD PLAINS

The geomorphology of the region has evolved over millions of years and has come to create the conditions for Hubbards to thrive. Gentle slopes, a meandering coastline, and a sheltered cove have provided access to pristine lands and rich waters for generations.

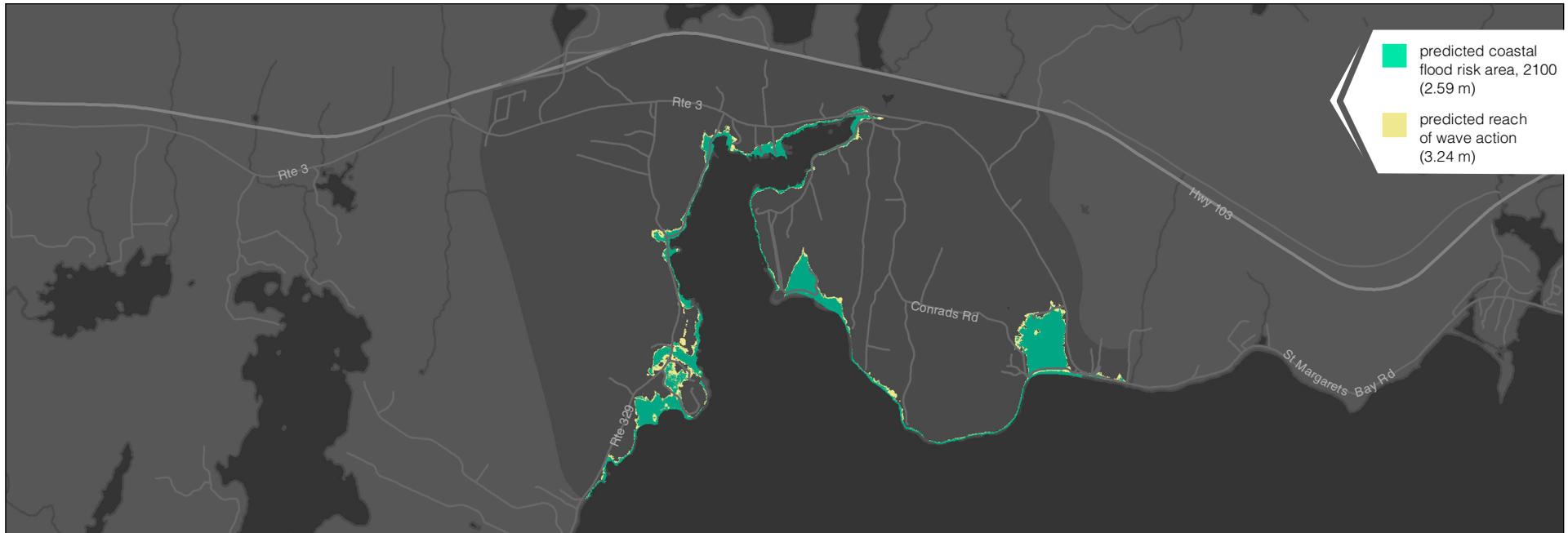
Despite the small project area, there is significant variability in the overall landscape in Hubbards. The highest elevations are located in the residential areas near Conrads Road, where three drumlin-like formations create a series of peaks and valleys.

On the opposite side of the harbour, the elevation rises to heights approximately 40 metres above sea level.

However, like many coastal communities in Atlantic Canada, Hubbards' low-lying coastal areas are exposed to rising sea levels and will come increasingly under threat as the climate continues to change. There are several large expanses of low-lying coastal lands that could become inundated or permanently flooded.

Queensland Beach, Hubbards Beach, and much of the coastline along Route 329 will be impacted by sea level rise.

Significantly, sea level rise estimates and mapping indicate impacts on key local transportation routes. Both St. Margaret's Bay Road and Route 329 could become either temporarily or permanently impacted by rising sea levels.



5.9 LAND OWNERSHIP

Most of the land in Hubbards and within the project area is privately owned. The second-most commonly found type of land ownership in Hubbards is provincial. The Province of Nova Scotia owns properties including portions of the Rum Runners Trail and Queensland Beach Provincial Park.

With portions of Hubbards being located in both the Municipality of the District of Chester (MODC) and Halifax Regional Municipality (HRM), these two municipalities also have land holdings in the project area. MODC owns the land

which is home to the Aspotogan Heritage Trust while HRM owns several properties in the project area including the Hubbards Recreation Centre and Elementary School.

Non-profit organizations also hold a significant number of properties in the project area, many of which front directly onto St. Margaret's Bay Road. Properties of two religious organizations have a sizable footprint in Hubbards, including 'Bishop's Park' at the mouth of the Hubbards River.

The public waterfront was bought by a community group in 2012. This

Community Waterfront Association was formed and purchased the land to preserve public access to the water when the former fish plant went for sale. The association needed to take on a mortgage for this purpose, and is paying of this debt through income from boat mooring and storage fees.

The Federal Government, which formerly had a significant presence in the area at CFS Mill Cove until it was divested in the 1990s, now holds two properties in the project area, including the post office.



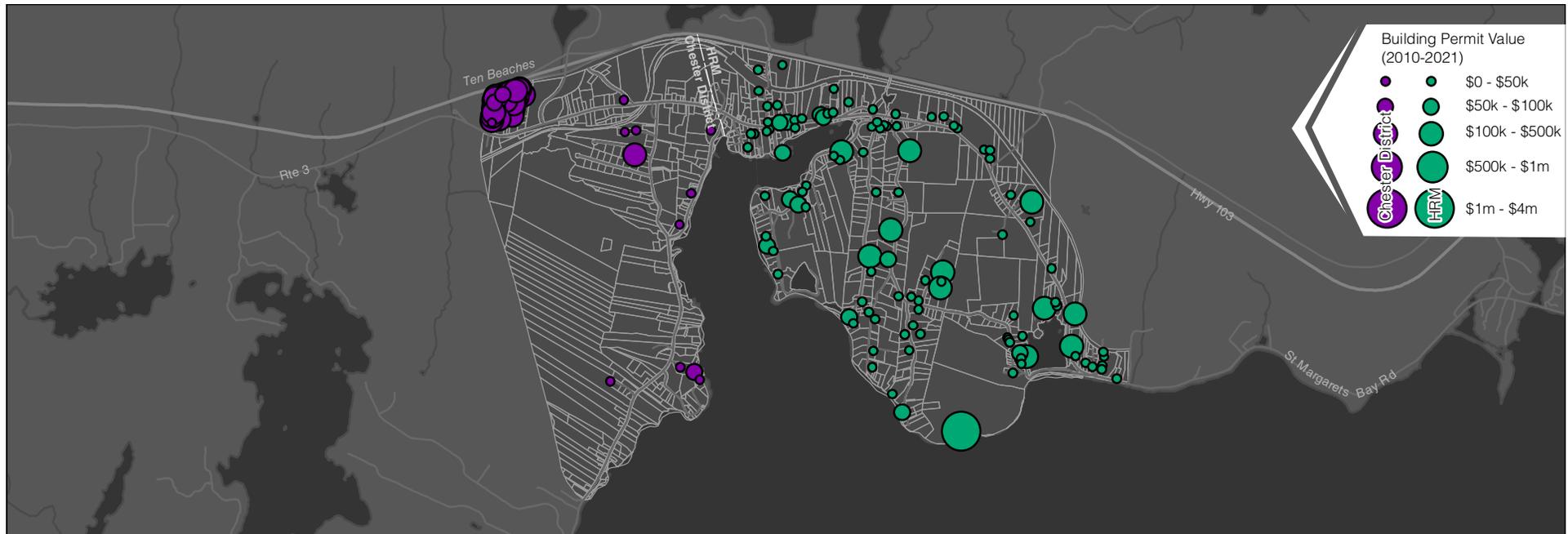
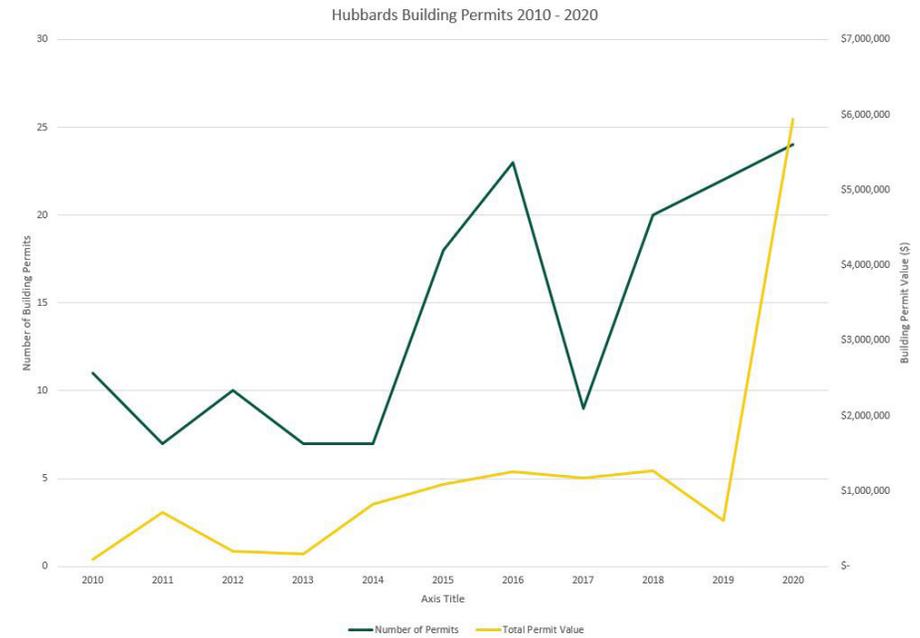
5.10 CONSTRUCTION ACTIVITY AND HOUSING

Over the last ten years, construction activity in the project area was stronger on the side of the Halifax Regional Municipality (HRM). The total value of construction in HRM was \$8.8 million, as compared to \$5.6 million in the District of Chester.

Within the Municipality of the District of Chester, the Ten Beaches mini home park was the main hub of building activity, and accumulated the vast majority of new housing starts in the area. Development in HRM was more distributed, with many smaller permits for repairs spread across the

area. There have also been more new constructions or major renovations spread over various locations of the HRM side of Hubbards.

The overall trend of building permit numbers is increasing. The years of 2016 and 2017-2020 saw approximately double the amount of permits issued than in the other years on the chart. However, the value of construction remains largely consistent. The stark increase of permit values in 2020 can be attributed to a single \$4 million residential construction on the Halifax side.



5.11 PLANNING CONTEXT IN THE HALIFAX REGIONAL MUNICIPALITY

The project area overlapping with the jurisdiction of the Halifax Regional Municipality (HRM) is located within the Municipality's 'Planning Districts 1 & 3 Plan Area.'

The planning approach in HRM is divided into 21 plan areas (Secondary Plans), which each have their own sets of land use bylaws and policies, specific to each community. Secondary Plans enable a finer-grained approach to municipal planning, and are often used to establish context specific policies and regulations for an area. All 21 of HRM's Secondary Plans are guided and informed by the Regional Plan, which is the long-ranging, municipal-wide planning strategy.

Within the Planning Districts 1 & 3 Plan Area, the project area has lands that are within the *Residential* Land Use Designation, the *Mixed Rural Residential* Land Use Designation, and the *Mixed Use "A"* Land Use Designation.

The Residential Designation is applied to those primarily residential areas along Conrads Road and Schwartz Road, which are almost entirely composed of single- or two-unit dwellings. At the intersection of Conrads and Shankle Roads is the singular location within the

project area that is designated as Mixed Rural Residential; a designation that is intended to facilitate a transition from a diversity of uses to predominantly residential uses. Finally, the Mixed Use "A" Designation, which is predicated on the semi-rural nature of the Plan Area, supports a wide range of residential, commercial and institutional uses.

Areas within the Residential Designation coincide with the Two Unit Dwelling Zone area. This zone prioritizes residential development in the form of one- and two-unit dwellings. The zone also permits day care facilities and businesses that are associated with a permitted dwelling. Seniors housing, institutional uses, and large home businesses (over 28 square metres in floor area) are permitted in this zone only by development agreement.

The Mixed Rural Residential Designation coincides with the Mixed Rural Residential Zone. In addition to permitting single- and two-unit dwellings, this zone also permits bed and breakfasts, craft and antique shops, personal service shops, grocery and convenience stores, and agricultural and forestry uses. Commercial uses are limited to 139 square metres of floor area.



Campgrounds, seniors housing, multi-unit dwellings (3 or more units) marinas, and crematoriums are permitted by development agreement in the Mixed Rural Residential Zone.

The Mixed Use 1 Zone permits a wide variety of uses, and instead of listing uses that are permitted, the zone lists uses that are prohibited. Mobile home parks and multi-unit dwellings are not permitted within this zone, except by development agreement. Similarly, campgrounds and commercial entertainment uses are not permitted in the Mixed Use 1 Zone except by development agreement. Commercial uses in this zone are limited to a maximum floor area of 697 square metres.

There are three pockets of land that are zoned General Industrial in the project area. The zone permits a range of residential, commercial, and resource uses. Single- and two-unit dwellings are permitted, as are mobile dwellings and home businesses. Manufacturing and processing uses over 278 square metres are permitted; so are office and retail uses that are associated with any permitted use.

Finally, the Regional Park Zone, which is limited in its application, is only applied to Queensland Beach Provincial Park. This zone does not

permit any development except for uses that complement the provincial park such as recreation uses, conservation uses, and public parks.

5.12 PLANNING CONTEXT IN THE MUNICIPALITY OF THE DISTRICT OF CHESTER

The project area overlapping with the jurisdiction of the Municipality of the District of Chester (MODC) is directly regulated by the Municipality's top-level planning strategy and land use by-law. Unlike in the Halifax Regional Municipality, MODC is not divided into secondary plan areas, except for one such secondary plan that applies to the Village of Chester. The remainder of the municipality is regulated under the overarching Municipal Planning Strategy.

Under the Municipal Planning Strategy, the project area falls within the *Settlement* Land Use Designation and the *Hamlet* Land Use Designation.

The Settlement Designation is applied to areas near Highway 103 and St. Margaret's Bay Road, and has more concentrated settlement patterns than in the rural areas of the municipality north of Highway 103. Comparatively, Hamlet Land Use Designation areas have distinct commercial and residential centres, and often have a density of uses that is greater than the Settlement Designation and in rural areas of the Municipality.

With regards to land use zones, there are three within the project area: the Settlement Residential Two Zone, the Mixed Use Zone, and the Hamlet Zone.

The Settlement Residential Two Zone prioritizes low density residential development (1-2 units on a lot), although more units are permitted through site plan approval (3-11 units on a lot) and development agreement (12 or more units on a lot). With the exception of a handful of uses that have the potential to create significant land use conflicts (abattoirs, fur farming, salvage yards, etc.), commercial uses with a floor area of 100 square metres or less are permitted. Commercial uses between 100 square metres and 500 square metres are permitted through site plan approval.

The Mixed Use Zone permits a greater number of residential units on a lot (1-4 units). Through site plan approval, 5-11 units can be developed on a lot, while 12 or more units are possible by development agreement. Within this zone, uses including campgrounds with fewer than 20 sites, marinas, recycling depots, and commercial uses with a floor area up to 2000 square metres are permitted. Commercial uses with a floor area greater than 2000 square metres is permitted, but only by development agreement.

Residential uses in the Hamlet Zone are permitted under the same restrictions as the Mixed Use Zone

(1-4 units are permitted by right, 5-11 units through site plan approval, and 12 or more units by development agreement). The Hamlet Zone permits a wide variety of commercial uses including tourist accommodations (up to 20 units) and campgrounds (up to 20 sites). Commercial uses with a floor area of 100 square metres or less are permitted and uses with a floor area between 100 square metres and 500 square metres are permitted through site plan approval.



6. COMMUNITY DEVELOPMENT VISION

6.1 COMMUNITY DEVELOPMENT MAP

The public dialogue on this project made it clear that it is important to residents of Hubbards to develop the area in a way which maintains 'community character'. This finding from the engagement phase of this project mirrors earlier engagement activities, which have—in a similar fashion—highlighted the need to preserve the community's identity and to control development expected to be triggered by the twinning of Highway 103.

However, there are reasons why these goals are difficult to achieve without clear guidance. While there is agreement that development should comply with the community's character, there seems to be no clear consensus on what exactly this character entails.

This may have to do with the fact that Hubbards is not a homogeneous settlement in the countryside. With a population of over 1,000 residents and 8.6 square kilometres within the project area, Hubbards is comparable to small towns across Nova Scotia in its size, structure and range of amenities. Consequently, the community features several distinct areas that have their own character. Questions about appropriate type of development have therefore to be

answered for each area separately rather than for Hubbards as a whole.

Moreover, the notion of trying to preserve a certain type of community character is generally problematic in urban planning, and Hubbards is no exception to this rule. No community is perfect, and residents of Hubbards have come forward to share their daily struggles that result from various shortcomings that persists in the area. Preserving the area 'as-is' would mean that all aspects of the community are indefinitely carried forward: the good and the bad ones.

We live in a complex and fast-paced world that is constantly evolving. Demographic, economic and societal developments keep challenging us, and require us to constantly rethink our ideals of community living. There is a growing desire to abandon cars for daily errands, and housing supply is currently perceived as inadequate compared to the needs of an aging population.

When speaking of goals for community development, it is important to create future ideals that we want to build a community on, as opposed to formulating goals that maintain a status quo. These ideals must be based on values derived from the community engagement, and they

must be tailored to specific locations in order to maintain relevance for the individual context of various neighbourhoods in Hubbards.

As a consequence of all of the above, the Hubbards Community Plan advances a total of eight community designations that are intended to guide the development within the project boundary. The designations are intentionally disregarding any legal and municipal boundaries to focus on what's essential: which areas within Hubbards can be grouped into categories based on their 'look, feel and function', and what are realistic goals for each of these areas' futures?

This plan will therefore advance the following eight community designations, further described and characterized on the following pages:

- + Community Centre
- + Rural Main Street
- + Community Facilities
- + Waterfront
- + Standard Residential
- + Trailfront Residential
- + Medium Density Residential
- + Natural Spaces

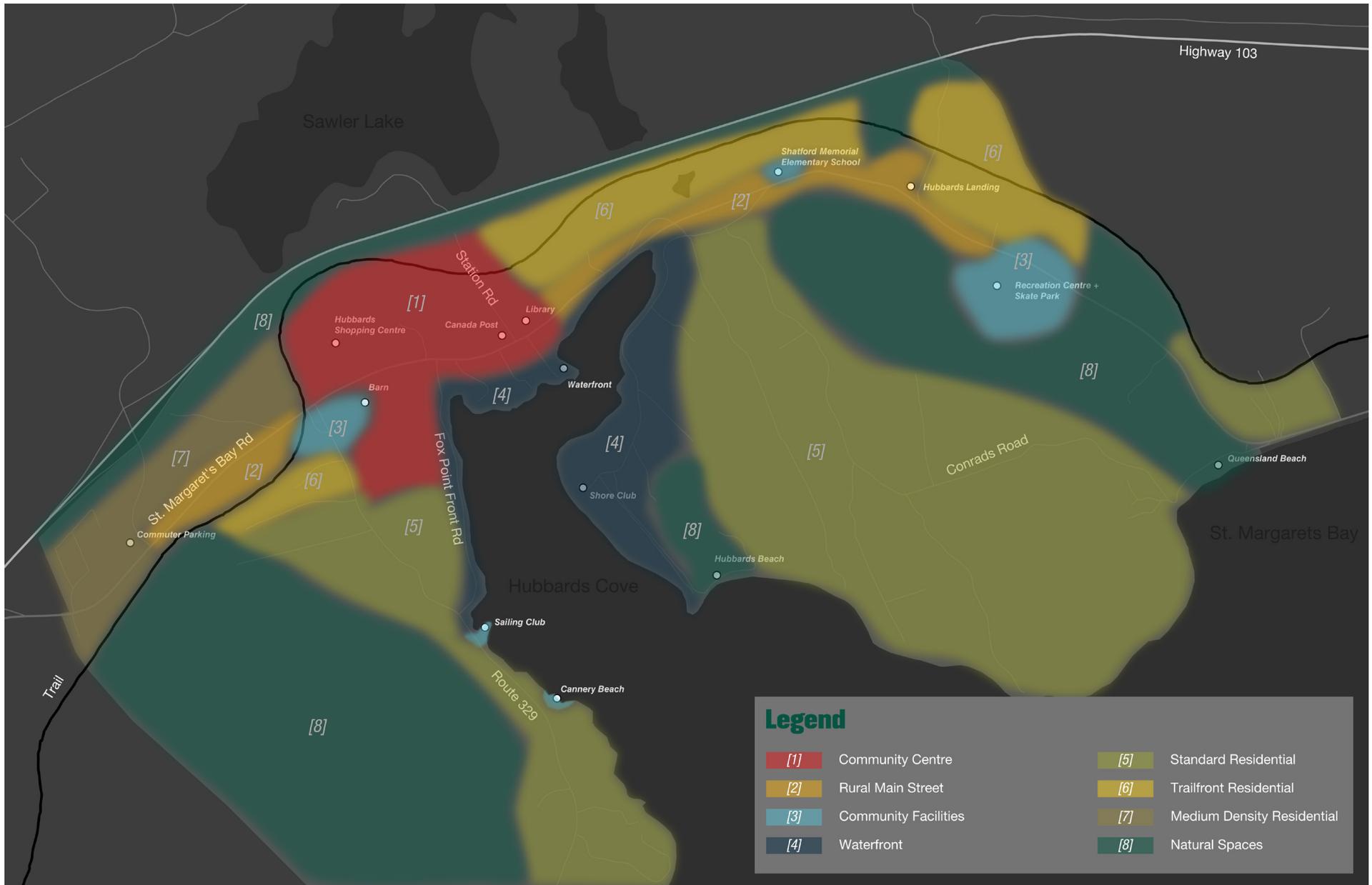


Figure 25: Community Development Map

6.2 COMMUNITY CENTRE DESIGNATION

DESCRIPTION

Prior to the arrival and proliferation of the motor vehicle for daily transportation purposes, communities used to often grow around train stations. The railway provided access to many amenities of nearby towns and cities, and community cores evolved within reasonable walking distances from these important transportation hubs.

The effects of this phenomenon still influence the land use structure of Hubbards to this day. Even though there were no trains in Hubbards since 1969, the area in proximity to the former train station is still characterized by a dense network of buildings standing on small lots. The resulting fabric of relatively continuous streetscapes is a characteristic unique to this central area. The proximity to many existing amenities further adds to this case.

The small lot divisions begin at 10324 St. Margarets Bay Road in the east and reach to the Aspotogan Heritage Trust building and trailhead in the west.

EXISTING AMENITIES

- + Post Office
- + Library
- + Cafes and Restaurants
- + Shopping Centre
- + Access to waterfront area

POTENTIAL

Being nestled between the former railway line and the waterfront, the area is the logical location for any future attempts to re-create a community core. Many roads in this area already offer a rhythm and pattern of buildings that is reminiscent of a traditional main street.

Sidewalks and small touches of landscaping and amenities on key corners can enable this area to become a vibrant community hub and accommodate a range of local boutique stores, restaurants and affordable housing that the community desires.

Architecture and land uses of the area should be generally aligned with the character of traditional town cores, offering opportunities for activity and interaction on the ground floor level.



Figure 26: Existing scenes in the area



Figure 27: Precedent community centre environment



Figure 28: Community Centre designation extent

6.3 RURAL MAIN STREET

DESCRIPTION

The Rural Main Street designation covers the portion of Saint Margaret's Bay Road outside of the Community Centre designation.

Development along this corridor is generally of an established rural residential character, with larger lot sizes and fewer commercial properties as compared to the Community Centre designation. Even though lots are generally deep and often extend right back to the Rum Runners Trail, most buildings are situated in proximity to the road, thus adding to the scenic character of the road.

EXISTING AMENITIES

- + Atlantic 12 Volt
- + Lola's Landing
- + Mr. Bean's Market
- + Hubbards Chiropractic
- + Hooked Rug Museum of North America
- + Access to Rum Runners Trail at the Aspotogan Heritage Trust
- + Hubbards Landing

POTENTIAL

While the residential character along the St. Margaret's Bay Road corridor is quite consistent, there are certain locations that appear suitable for infill development.

Furthermore, in the event that a multi-use path was to be constructed through Hubbards along the north side of St. Margaret's Bay Road, properties between the Community Centre designation and the Shatford Memorial Elementary School would be well connected to the community centre, unlocking potential for intensification in that area.

Commercial development along this corridor works well in locations where small, individual shops are set up along the road. This characteristic should be further built upon. The architectural standards of buildings should relate to the rural character of the area.

Design guidelines for buildings should promote the picturesque character of the community, and set minimum standards for property arrangements. Due to the rural character of properties in this designation, parking in the front yard of buildings can be seen as an acceptable practice, as long as the area is generally landscaped and avoids hardscaped pavements.



Figure 29: Existing scenes in the area



Figure 30: Precedent rural main street environment



Figure 31: Rural Main Street designation extent

6.4 COMMUNITY FACILITIES

DESCRIPTION

The Community Facilities designation is applied to five separate areas in Hubbards. A cluster of community facilities is centred around the intersection of St Margaret's Bay Road and Route 329.

One municipally owned property in this location accommodates offices of the Aspotogan Heritage Trust, the 'Through The Years' Daycare, a visitor information centre and other facilities. Adjacent to that is the Hubbards Fire Station. Across the street, the Aspotogan Heritage Trust owns the property containing the Hubbards Barn. This is the largest, most densely developed of the five areas.

Near to the eastern end of Hubbards is another cluster of community facilities that includes the Recreation Centre and the Hooked Rug Museum. The Recreation Centre is integrated into a wooded landscape on the south side of the road, and features woodland trails, a skatepark, two ball fields, hard courts, and further amenities.

The Community Facilities designation is also applied to Shatford Memorial Elementary School, the Sailing Club, and Cannery Beach.

EXISTING AMENITIES

- + Cannery Beach
- + Hubbards Sailing Club
- + Aspotogan Heritage Trust
- + Hubbards Barn Association
- + Pharmacy and other community health facilities
- + Shatford Memorial Elementary School
- + Recreation Centre and Skate Park

POTENTIAL

With the daycare, community gardens, and heritage facilities already located here, the cluster of community facilities around the Aspotogan Heritage Trust is well positioned to become established as a western gateway to the community and centre for intergenerational learning. There are opportunities for non-market housing options along the east side of Route 329.

Together, the Recreation Centre and Hooked Rug Museum provide the foundations for a vibrant, diverse, and intergenerational sports and culture hub which could be expanded to include new athletics, arts, and cultural destinations. The hub would be ideally suited for integration with the Rum Runners Trail.



Figure 32: Existing scenes in the area



Figure 33: Precedent of community facilities (Image credit: RHAD Architects)



Figure 34: Community Facilities designation extent

6.5 WATERFRONT

DESCRIPTION

A defining feature of Hubbards is its spectacular waterfront, visible from all sides of Hubbards Cove. The waterfront is framed by three roads: Fox Point Front Road, St Margaret's Bay Road, and Shore Club Road. Most of the properties along the Hubbards waterfront are privately owned, with buildings including single-unit residential, seasonal residential, condominium residential, commercial, and institutional uses. Some sections of the waterfront are publicly accessible, but large sections of private lands limit the waterfront access.

In addition to the properties fronting directly onto the shoreline, this designation includes properties located along the east side of Shore Club Road where two multi-unit condominium developments are found, along with a 127-site campground and the Shore Club itself.

EXISTING AMENITIES

- + Wharves, Slipways, and Piers
- + Public Housing
- + Hubbards Community Waterfront
- + Restaurants and Cafes
- + Campground
- + Shore Club

POTENTIAL

Participants of public discussions about the Community Plan have suggested various forms of improved waterfront access. Overall, there is a clear desire to improve the integration of the waterfront into the community fabric and daily lives of residents.

Policies for the area will need to reflect this circumstance, and aim to promote a type of development that reduces some of the restrictions, as they relate to waterfront access. On residential properties, this will only be partially possible. However, on commercial properties there should be opportunities to integrate businesses into public walking routes, with mutual benefits for the public and business owners alike.

Similarly to the community centre designation, development on the waterfront should follow high design standards, as buildings along the water decisively impact the views within Hubbards Cove. Some of the most essential view planes onto the water, from key points in the community, should be identified and secured through view corridors in land use planning documents.



Figure 35: Existing scenes in the area



Figure 36: Precedent of waterfront utilization



Figure 37: Waterfront designation extent

6.6 ESTABLISHED RESIDENTIAL

DESCRIPTION

The St Margaret's Bay Road corridor and the waterfront notwithstanding, the majority of the residential areas in Hubbards are located within established neighbourhoods. Land uses are largely homogenous in these parts of Hubbards, with most lots being residential and the single family dwelling being the dominant form of housing.

These areas are also characterized by relatively large rural lots, extensive tree canopy, and a series of narrow country roads that wind through the community. Many houses are tucked away behind thick vegetation that lines the roads, which add to the rural character of these areas.

EXISTING AMENITIES

- + Home-based businesses
- + Forest
- + Cemetery
- + Access to Queensland Beach

POTENTIAL

These areas are unlikely to undergo any significant changes over the mid-term and therefore need little guidance in terms of their development prospects. With many residential homes scattered through this area, property ownership and redevelopment are expected to happen incrementally.

With many private residences present in these areas, conflicting land uses that emit noises, smells or excessive amounts of traffic should generally be avoided in these neighbourhoods.

In order to provide a contribution to remedy the current housing shortage, many lots could accommodate some infill development in the form of backyard, garden and garage suites. These units could be e.g. offered on the rental market.

Some large vacant lots are also present in the area, and could potentially absorb slightly higher density development such as cottage condominiums or low-rise apartment buildings. Due to the absence of a municipal wastewater system in Hubbards, an increase of residential densities through smaller lot sizes would only be possible to a very limited extent.



Figure 38: Existing scenes in the area



Figure 39: Example garden dwellings (Image credit: Sightline Institute, CC BY-SA 2.0)



Figure 40: Established Residential designation extent

6.7 TRAILFRONT RESIDENTIAL

DESCRIPTION

The Rum Runners Trail was built on the lands of the original railway line that connected Halifax to Bridgewater and Lunenburg. Today, the corridor is finished as a multi-use trail with a crushed stone surface suitable for walking and cycling.

Through Hubbards, lands around the trail are largely left in their natural state and the trail is framed by forest on both sides. This causes long-distance trail users to be often unaware that they are moving through Hubbards and the lack of landmarks makes navigation difficult. The seclusion also creates safety concerns, and is one of the main reasons why the trail is not used as much by local residents as it could be.

While ecologically valuable habitats along the trail should be protected, some stretches of the trailfront could potentially accommodate some level of development.

EXISTING AMENITIES

- + Access to Queensland Beach
- + Bridge over Hubbards River
- + Natural Environment

POTENTIAL

Part of the lands along the trail will likely get developed for residential purposes at some point. These properties could be required to include trail-oriented front elevations that enhance the trail with attractive building designs. Within a buffer of five metres from the trail, there could be specific landscaping requirements. Large blank walls, chain-link fences and parking along the trail should not be permitted. Car access to such properties would occur through driveways coming from service alleys in the rear of homes.

This type of development could make a significant contribution to promoting active lifestyles in the community. The homes would function as a natural source of supervision on the trail, alleviate safety concerns and solidify the trail as the backbone of active transportation in the community.

Cafés, shops or mixed-use developments that combine businesses with residential uses could further increase the activity around the trail. The development of an “Artisan Alley” could include art gallery spaces, food establishments, light industrial uses, and live/work studio spaces at ground level.



Figure 41: Typical trail section in Hubbards



Figure 42: Vision for trailfront residential development



Figure 43: Trailfront Residential designation extent

6.8 MEDIUM DENSITY RESIDENTIAL RESIDENTIAL

DESCRIPTION

The majority of lands in this part of the community are wooded and vacant. However, the 'Ten Beaches Estates' mini home park sets an interesting precedent for somewhat higher density residential development in this corner of Hubbards, as it currently accumulates 51 homes on a lot of only 17 acres. If development was extended at that rate, this would result in a level of population density similar to parts of Dartmouth and Cole Harbour.

The area is bisected by Mill Lake No. 1 Road, which is the primary route into Hubbards from Highway 103. If lands on either side of this road get developed, this will be the place where many visitors gain their first impression of the community.

EXISTING AMENITIES

- + Commuter parking lot
- + Proximity to the Rum Runners Trail
- + Business and directional signage at the intersection

POTENTIAL

The area between the Ten Beaches mini home park and the Hubbards Shopping Centre could be a suitable location for more developments of medium residential densities.

However, mini home parks are not necessarily compatible with the traditional village identity which residents of Hubbards value, both in terms of architecture and site planning. While medium density developments which maintain the rural character are rare in Nova Scotia, they do exist elsewhere and could be adopted: building types such as courtyard housing or grouped cottages could maintain the rural character of Hubbards and provide an affordable housing option at the same time.

The location seems suitable for affordable and seniors housing, since the proximity to the shopping mall, daycare and other amenities could even allow for car-free access to basic needs of families and individuals alike.

It is possible to imagine that, in time, Hubbards may get access to some form of a regional transit network. The existing commuter parking lot could function as a multi-modal transportation hub in such a scenario, and lands immediately surrounding it would be a perfect location for transit-oriented development. One long distance bus route from Halifax to Lunenburg does already stop at the Hubbards Shopping Centre.



Figure 44: Small but dense: this grouped cottage development fits six residential units while using very little space.



Figure 45: Hubbards already features one cottage condominium development, even though this example targets high-end market buyers.



Figure 46: Apartment buildings could be an option for this area (e.g. nearby the commuter parking lots), if their design matches rural architecture.



Figure 47: Medium Density Residential designation extent

6.9 NATURAL SPACES

DESCRIPTION

The community of Hubbards has rich natural assets. Millers Marsh is a large wetland system at the end of Hubbards Estates Road. Another smaller wetland is situated between the Hubbards Landing development and the Rum Runners Trail.

There are three sandy beaches within the project boundary of this community plan, and two of them coastal sand bars backed by a barachois, i.e. a lagoon where salt and fresh water blend.

The Wight Nature Preserve is another ecological asset in the area: this mix of woodland and bog was donated to the Province in 1975 and has been largely free from human interference since. With time, an old growth forest will form here.

Finally, there is a stretch of woodlands between Highway 103 and the Rum Runners Trail which creates a natural buffer between the noisy transportation route and the community. Some residents already complain about noise exposure from the highway. If the buffer of trees was ever thinned or removed, it would either need to be replaced by expensive noise barriers or the quality of life would decrease in Hubbards.

EXISTING AMENITIES

- + Forest
- + Wetlands
- + Streams
- + Hubbards River
- + Dauphinees Lake
- + Schwartz Lake
- + Hubbards Beach
- + Queensland Lake and Beach
- + Wight's Nature Preserve

POTENTIAL

The main goal for these areas is preservation and protection from development. With the right level of environmental stewardship, the community will be able to enjoy these natural features for generations to come, including their recreational and utilitarian values.



Figure 48: Dauphinees Lake is a barachois pond behind Hubbards Beach



Figure 49: A buffer of trees protects the residents of Hubbards from Highway 103 noise.



Figure 50: Natural Spaces designation extent



7. CONNECTIONS AND BIG MOVES

7.1 ACTIVE TRANSPORTATION NEEDS

Now that the plan identified a desired future state of the community as a whole, necessary transportation linkages are beginning to emerge and can be summarized in the following categories:

MAIN CORRIDOR

The majority of community features and central areas for development are lined up on a stretch of St. Margaret's Bay Road. The entire centre of the community and all of its amenities are entwined around this artery, which also connects this centre to two important community facilities: the Aspotogan Heritage Trust building and the J.D. Shafford Elementary School.

Since Hubbards is divided into a western and eastern part through the waters of Hubbards Cove, all land-based transportation is also forced to pass through the St. Margaret's Bay Road corridor in order to travel between these two sides of the community. Consequently, the availability of a solid active transportation spine within this main corridor is a basic pre-requirement to enable any meaningful active transportation connectivity throughout Hubbards.

While the Rum Runners Trail could potentially offer an alternative route

between the two shores of Hubbards Cove, community engagement has shown that this piece of infrastructure has—at least for the time being—limited application for daily usage. Many residents perceive the trail as unsafe due to its relative remoteness and seclusion. In the foreseeable future, the trail will not be an adequate replacement for walking and cycling infrastructure on St. Margaret's Bay Road.

Creating opportunities to safely walk and bike through the community core will also have another advantage: trail users can be drawn into the community, benefiting various businesses. The trail routing generally leads through natural habitat and creates little encouragement to actively explore the interior of Hubbards. Dedicated biking infrastructure has the potential to attract more visitors to the community core and to promote Hubbards as a key destination on the Rum Runners Trail.

Various studies have shown that bicycle tourism is a significant source of economic activity.

A recent study of tourist behaviour along the 'Route Verte' in Quebec reveals that bicycle tourists spend on average more money on their trips

than any other group of tourists.¹ Consequently, better linkages between businesses on St. Margaret's Bay Road and the trail will help the local economy to support that vibrant main street environment which the community desires.

WATERFRONT CONNECTIONS

The second-most important active transportation connections for Hubbards are those surrounding Hubbards Cove. Being a Maritime community, residents of Hubbards feel a connection to the sea and wish for more opportunities to explore the waterfront by foot.

At the same time, the waterfront contains a number of important destinations for locals and visitors alike. Shore Club Road has been highlighted as a dangerous roadway during the public consultations. It is lined by numerous businesses and forms a transportation bottleneck to Hubbards Beach and the Shore Club, which are both anchor institutions in the community. On the western shore of Hubbards Cove, safe access to the Sailing Club is a key community concern.

¹ http://veilletourisme.s3.amazonaws.com/2015/04/Velo_tourisme_Sommaire_executif.pdf

ADDITIONAL LINKS

The need for safe walking and biking connections does not end with these measures. Further desirable links include a connection between the community core and the hub of recreational facilities near the Hooked Rug Museum. Much of this connection can be established as a greenway through the Wight Nature Preserve and other Provincial lands, although some minimal crossing of private land would have to be negotiated as well.

Further desirable links include active transportation routes along Route 329 and more cross connections between St. Margaret's Bay Road and the Rum Runners Trail. The recent formalization of the Vinegar Lake Road right-of-way offers an opportunity to connect the trail to the Hooked Rug Museum and the Recreation Centre across the street, if a crosswalk is installed in that location.

Strategic land acquisitions could help with better integration of two key beaches into the active transportation system. Queensland Beach could be accessed through a short connector from the Rum Runners Trail, while Hubbards Beach could potentially be connected to Conrads Road.

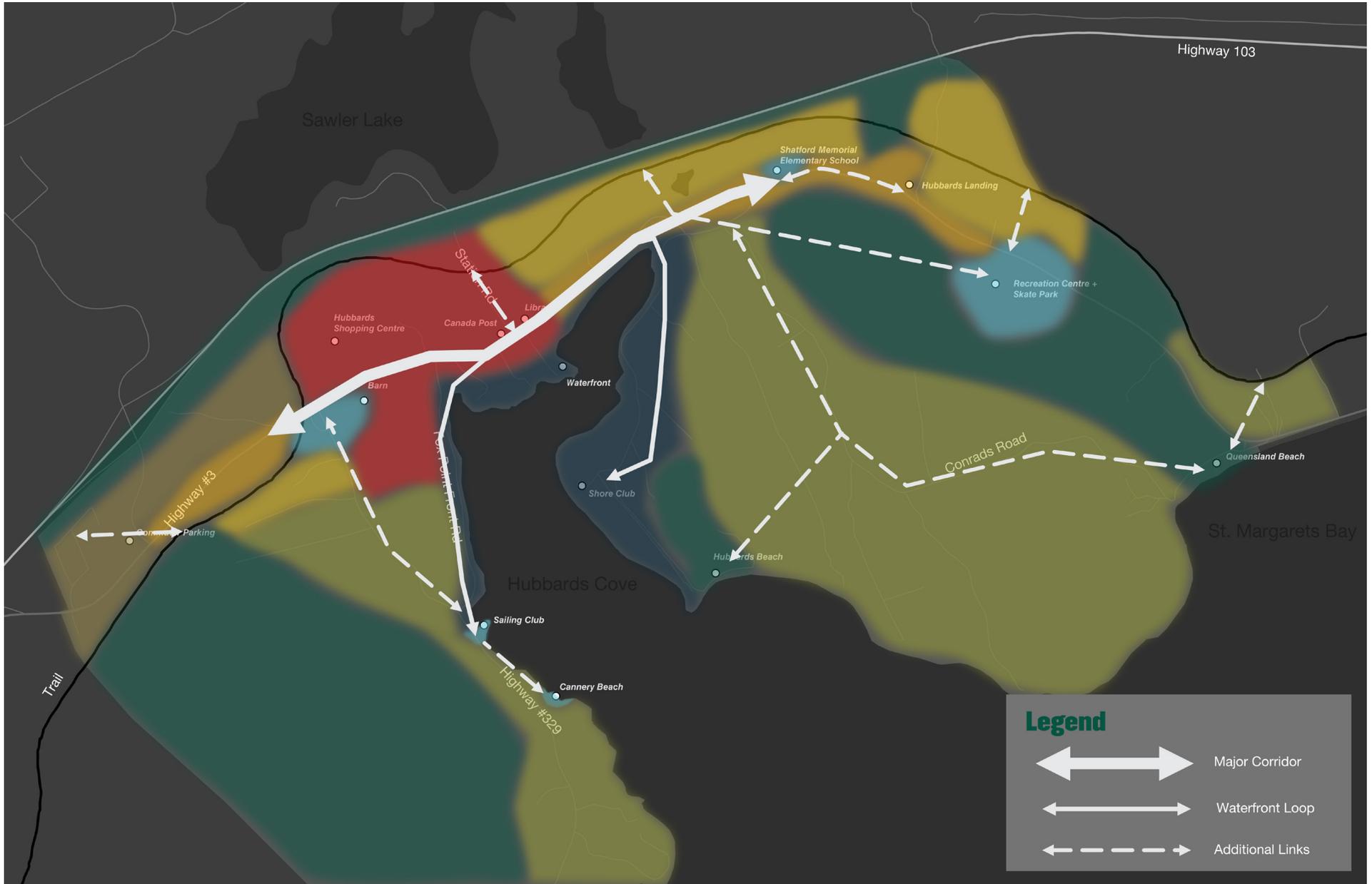


Figure 51: Main Active Transportation Needs in Hubbards

7.2 SIX BIG MOVES

Based on the analysis of the current community structure, its desirable future form as well as the active transportation needs evident from this analysis, the Hubbards Community Plan generally suggests the following 'Six Big Moves' for the community:

THE 'HUBBARDS LOOP'

St. Margaret's Bay Road is the main corridor leading through the community, and it therefore requires safe options for both walking and biking. Start and end points of streetscape improvements make the most sense where they create links to the Rum Runners Trail.

The elementary school has recently received an active transportation upgrade by means of a connector link to the trail. This is therefore a logical starting point to begin a trail bypass, and to invite bicyclists to explore the centre of the community, before joining back to the trail at the current trail head near the Aspotogan Heritage Trust property.

In order for this trail to be usable by pedestrians as well, this route should be arranged as a multi-use path. The majority of distance between the two trail access points is of residential character, and does not necessarily require an environment

that is fully focused on pedestrians. Allowing pedestrians and bicyclists to mix on that path will allow to improve traffic safety for both groups using one infrastructure investment.

Creating a bypass to the Rum Runners Trail has also the added advantage of creating an active transportation loop through the community, which would likely become very popular with joggers and other sports enthusiasts. This infrastructure upgrade will therefore be referred to as the 'Hubbards Loop' throughout the Community Plan.

SIDEWALK EXTENSION

Within the denser area identified as 'community centre' on the community vision map, a multi-use path alone is not sufficient to create the type of main street which supports the creation of strong business environment and community gathering places.

Therefore, the currently existing sidewalk should be extended from Fox Point Front Road until slightly beyond Yacht Club Road, so that it reaches the upper section of the community waterfront park in proximity to the amphitheatre. This will allow for pedestrian access to the community waterfront through this little-used part of the waterfront

park, keeping pedestrians away from Yacht Club Road, which sees frequent traffic of trucks with boat trailers and which is also used for parking.

Another short but vital piece of sidewalk is needed across the bridge over Hubbards River. The lack of clear traffic lanes or shoulders on the bridge and the vaguely defined multi-street intersections on both sides of the bridge make this a particularly uncomfortable spot for pedestrians.

CENTRAL PLACEMAKING

In order to create a community core which entices the local community to walk and to support small, local businesses, the built environment does not only need to be safe but also interesting. In other words: safety is a precondition for walking in the community, but walking only makes sense if there are destinations to go to. Therefore the creation of pleasant and interactive places must be integral to any streetscape improvement considerations.

There are several properties within the community centre that contain underutilized sites and could accommodate a mix of landscaping, new street furniture and community amenities. However, this plan avoids suggestions for

placemaking on private property and focuses on lands owned by community groups public institutions and Crown corporations instead.

In this context, three locations stand out as opportunity sites for placemaking improvements:

- + the upper part of the community waterfront
- + the lawn in front of the post office
- + the corner of Route 329 and St. Margaret's Bay Road

The first site has great potential to enhance the streetscape of St. Margaret's Bay Road. Owned by the community waterfront association, this lot has about 50 metres on frontage along this artery. While the lower portion of the property faces multiple operational constraints and awaits engineering assessment for the eroding pier, the upper portion around the existing amphitheatre offers opportunities for immediate placemaking.

In close proximity to the first site, the corner of St. Margaret's Bay and Station Roads includes large lawns in front of the post office and library, which can accommodate various amenities that would add to the visual identity of Hubbards and

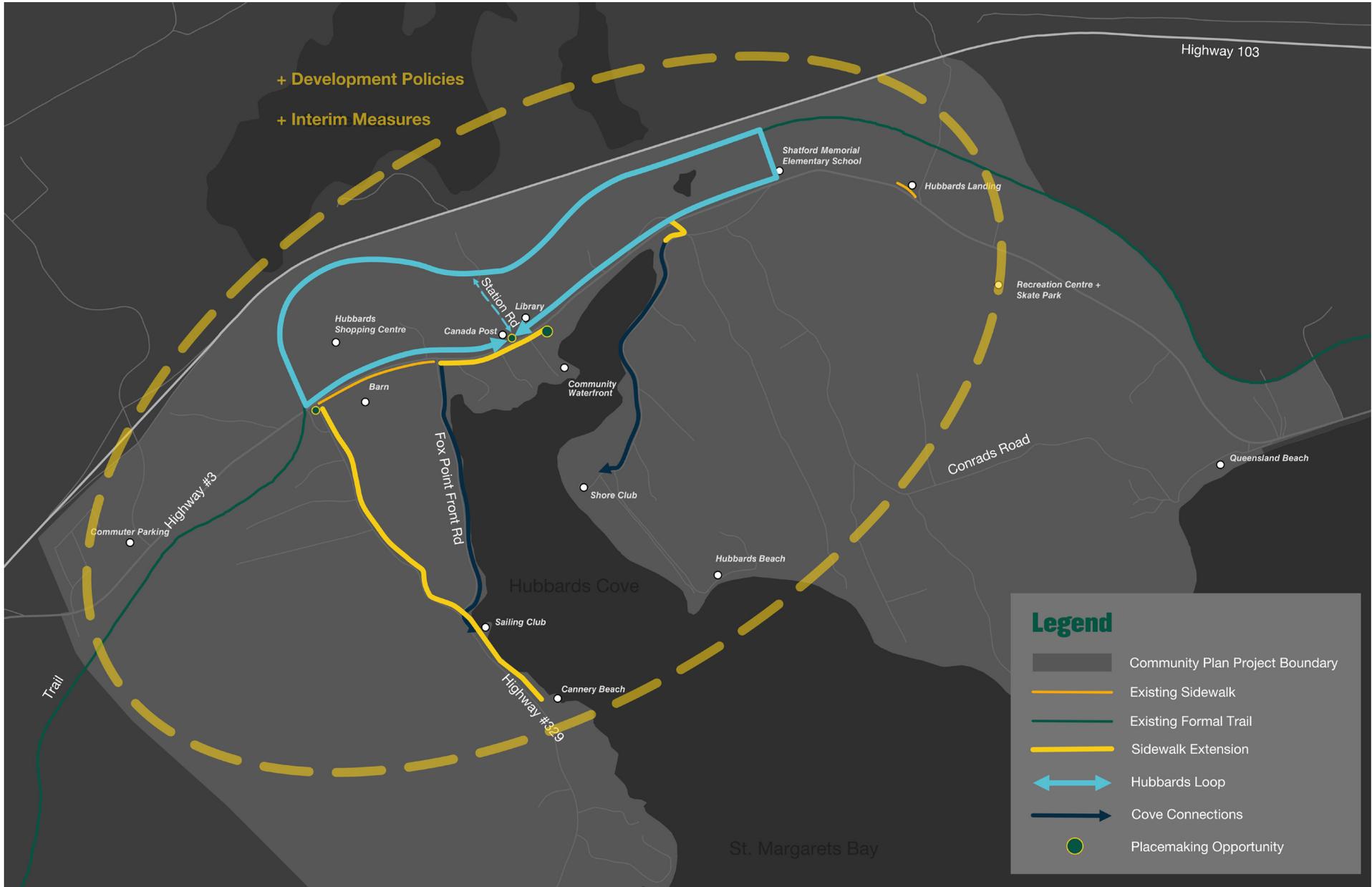


Figure 52: Six Moves Map

add to active transportation features leading in front of them. Finally, the property of the Aspotogan Heritage Trust on the corner of St. Margaret's Bay Road and Route 329 has potential for another recognizable location in Hubbards. The main building on this property accommodates the offices of the Aspotogan Heritage Trust and various community facilities. The land already accommodates a community gardening project, which could be framed by additional landscaping feature to become a small park.

COVE CONNECTIONS

The analysis of active transportation needs in the community showed the need for pedestrian connectivity around Hubbards Cove.

On the western shore of the cove, the Municipality of the District of Chester is already working on a feasibility study analyzing several options of change to the configuration of Fox Point Front Road. In all scenarios, the changes are intended to increase room for pedestrians, calm traffic or accomplish both of these goals. The implementation of the chosen measure for this road will constitute one part of this cove connection.

On the eastern shore of the cove, Shore Club Road is in need for

upgrades to facilitate pedestrian traffic from and to the Shore Club and Hubbards Beach. As part of the 'Six Big Moves', a boardwalk along the shore would make a great difference for pedestrian safety on this stretch of the road. Additionally, a road realignment in proximity to the Tuna Blue Inn would help to make room for pedestrians at a dangerous curve with limited visibility.

PROPOSED POLICIES

The Community Development Map identifies characteristic areas of Hubbards, and the Community Vision formulates desirable types of development for these areas.

Most of these areas are privately owned by various individuals and businesses. The Hubbards Community plan is a document providing guidance, and has no legal authority to set rules for development on private properties. However, this plan can suggest appropriate development policies and encourage municipalities to consider their adoption. Consequently, the Hubbards Community Plan brings forward a set of proposed development policies, which can be adopted by both the Halifax Regional Municipality and the Municipality of the District of Chester.

INTERIM MEASURES

Finally, many of the measures advanced by this plan have a relatively long horizon for implementation. However, conversations with the community have shown that there is a wide-spread sense of urgency to improve the safety of pedestrians and bicyclists in the community.

This perceived urgency for action is not entirely surprising. After all, Hubbards' size, population and range of amenities are comparable to smaller Nova Scotian towns, but unlike those towns Hubbards has only one short stretch of sidewalk which does not connect to any community destinations. As a result of this, there is a clear threat to residents' safety, especially those who cannot drive such as children and teenagers.

Consequently, as many active transportation infrastructure gaps as possible should be remedied in the interim through low-budget improvements which reduce some of the largest safety risks for pedestrians, and possibly test some concepts which can be evaluated prior to development of permanent designs.



8. STREETScape INVENTORY

8.1 BASIC ANALYSIS OF ST. MARGARET'S BAY ROAD

ROAD PROFILE

The typical cross section of St. Margaret's Bay Road consists of approximately 20m within the right of way.

The roadway features one travel lane per direction with widths of 3.5 metres each. The road surface is flanked by gravel shoulders of varying widths, resulting in overall road widths of approximately 8-11 metres. This means that at most locations, half of the right of way is unused and could accommodate additional features.

Two stretches of the road feature short sections of sidewalk. Municipal sidewalk exists from Route 329 to Fox Point Front Road. Stormwater is managed with catch basins beneath the curb within this section.

Another short sidewalk was provided on a singular private property: Hubbards Landing. This sidewalk was apparently installed in anticipation of further development around that site. However, this prospect has not materialized and the sidewalk does not connect to anywhere. Both the 'Landing' and the sidewalk remain somewhat isolated in this location. No means of stormwater management were provided with this particular sidewalk.

CROSS SECTIONS

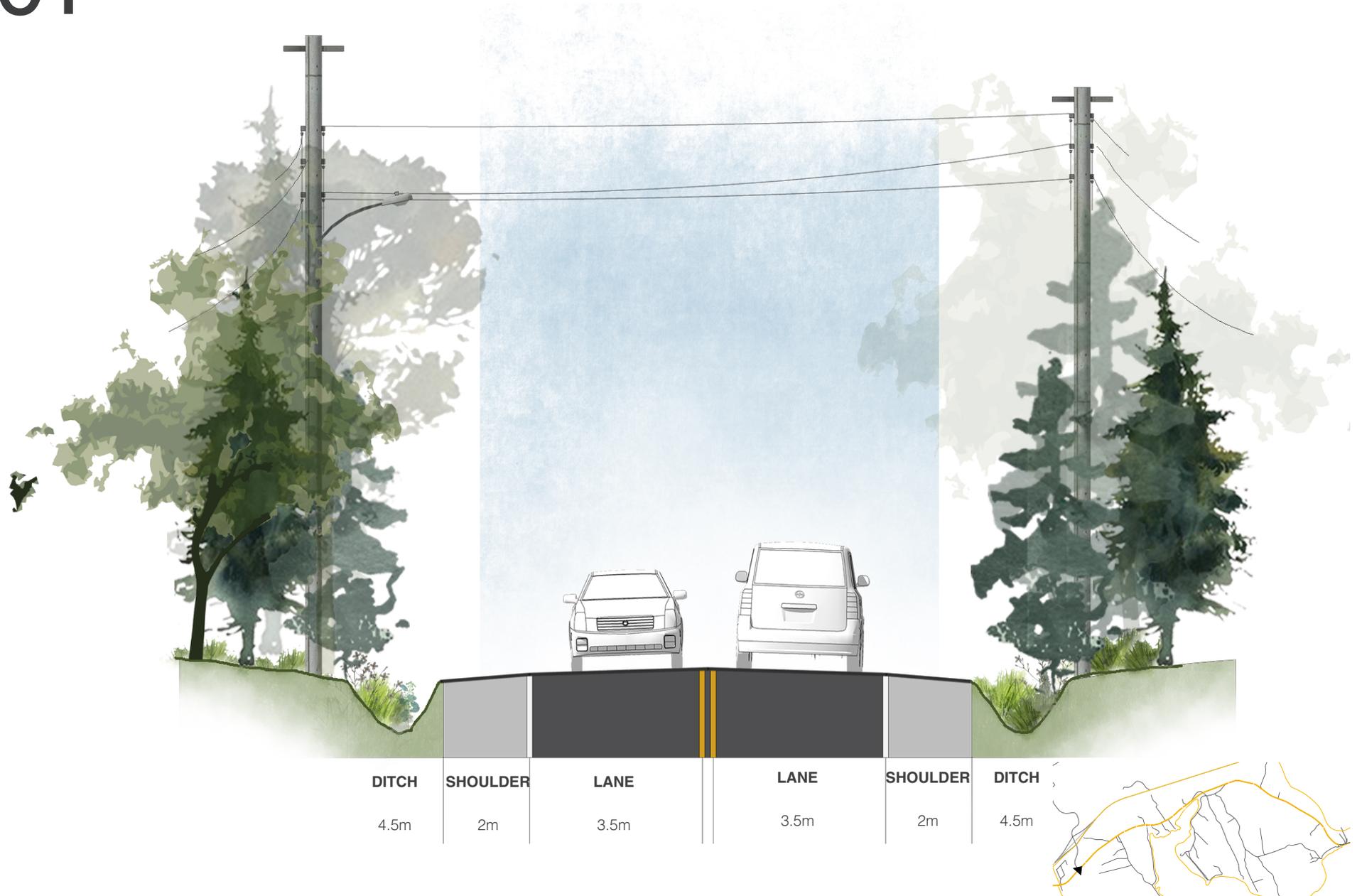
Analyzing the cross section of St. Margaret's Bay Road, a total of eleven distinct cross section types emerges. The following road stretches with specific right of way configurations can be identified when crossing through Hubbards:

- + **Section 01: Mill Lake Road No. 1 (Exit 6 Connector) to Civic Number #233.** This section of road features a narrow shoulder flanked by naturalized ditches on each side of the road
- + **Section 02: Civic Number #233 to Highway #923.** Most parts of this stretch only include a naturalized ditch on one side of the road. The other side transitions flush to adjoining properties.
- + **Section 03: Route 329 to Barn Driveway.** The only continuous stretch of sidewalk begins in this section. The other side of the road is interrupted by some large driveways and parking areas, but generally slopes gently to neighbouring properties.
- + **Section 04/05: Barn Driveway to Fox Point Front Road.** Sidewalk continues on southern side of road, while a naturalized ditch forms on the northern side of the right of way. In some instances that ditch is interrupted by parallel parking (see Section 05)
- + **Section 06/07: Fox Point Front Road to J.D. Shatford Memorial Library.** Two types of cross sections are alternating in this area. There is no more sidewalk past Fox Point Front Road when heading east, and many areas of this road have irregular driving and parking areas merging into the shoulder, just as shown on Section 06. Other areas—mostly those with an inclining or declining grade in the roadway—feature one naturalized ditch and a gravel shoulder on the other side (see Section 07).
- + **Section 08: Civic #10308 to Hubbards River Bridge.** Heading further east, the scenery changes soon after the library. The residence with the civic number 10308 is the last building on the southern side of the road. From here onwards, the shoulder starts sloping steeply down to Hubbards Cove. The Cove narrows down eastwards and eventually becomes Hubbards River, while the cross-sections remain largely unchanged within this stretch.
- + **Section 09: Hubbards River Bridge to Hubbards Landing.** East of the river, ditches reappear on one side of the road. Most development can be found on the northern side of the road, while the southern side often slopes into surrounding woodlands and wetlands. This situation is mirrored just before Hubbards Landing, where the wetland is situated on the northern side of the road.
- + **Section 10: Hubbards Landing.** The streetscape around this mixed-use development on the corner with Old Rock Road is unique in that it features a short 170 metres of sidewalk in front of the property.
- + **Section 11: Hubbards Landing to Queensland Beach.** Looking east, this final section of St. Margaret's Bay Road through Hubbards is very rural in character, and shows sparse development along largely. Various types of ditches are present in this section of the road. In some cases, the shoulder simply transitions into lower lying woodlands.



Figure 53: Overview of existing street cross-section types

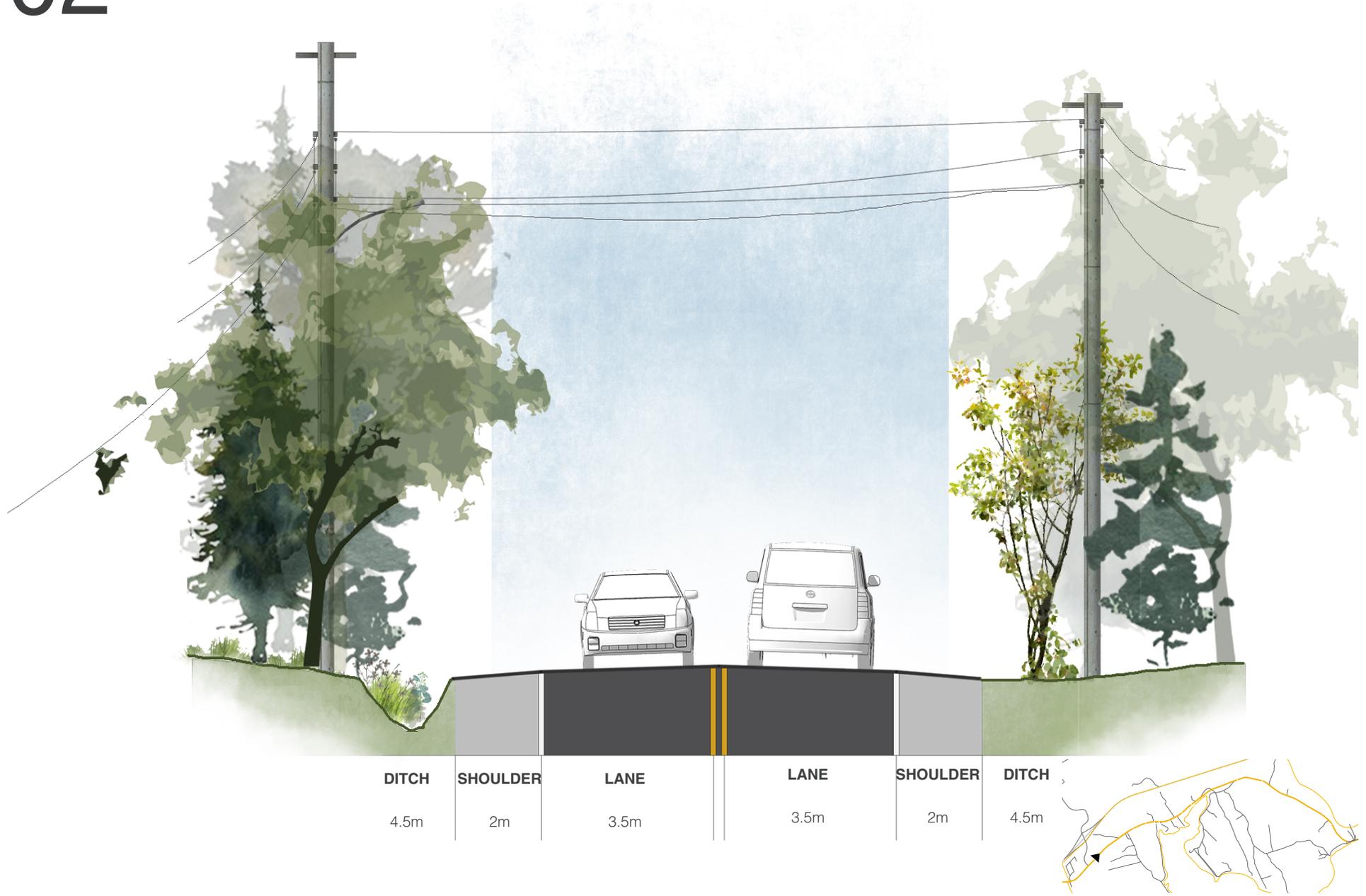
01



MAXIMUM
70



02





Naturalized ditch

Community signs

Driveway + parking area blends with the shoulder of the road

Mail box

Utility Poles & Overhead Wires

03



MAXIMUM
50

Utility Poles
& Overhead
Wires

Crosswalk sign
on wires

Local
business signs

Naturalized ditch

Driveway + parking
area blends with the
shoulder of the road

Sidewalk with grass verge

Curb is built into
with sidewalk

Sidewalk on one
side of the road



04



MAXIMUM
50

Overhead Utility
Poles & Wires

Naturalized ditch

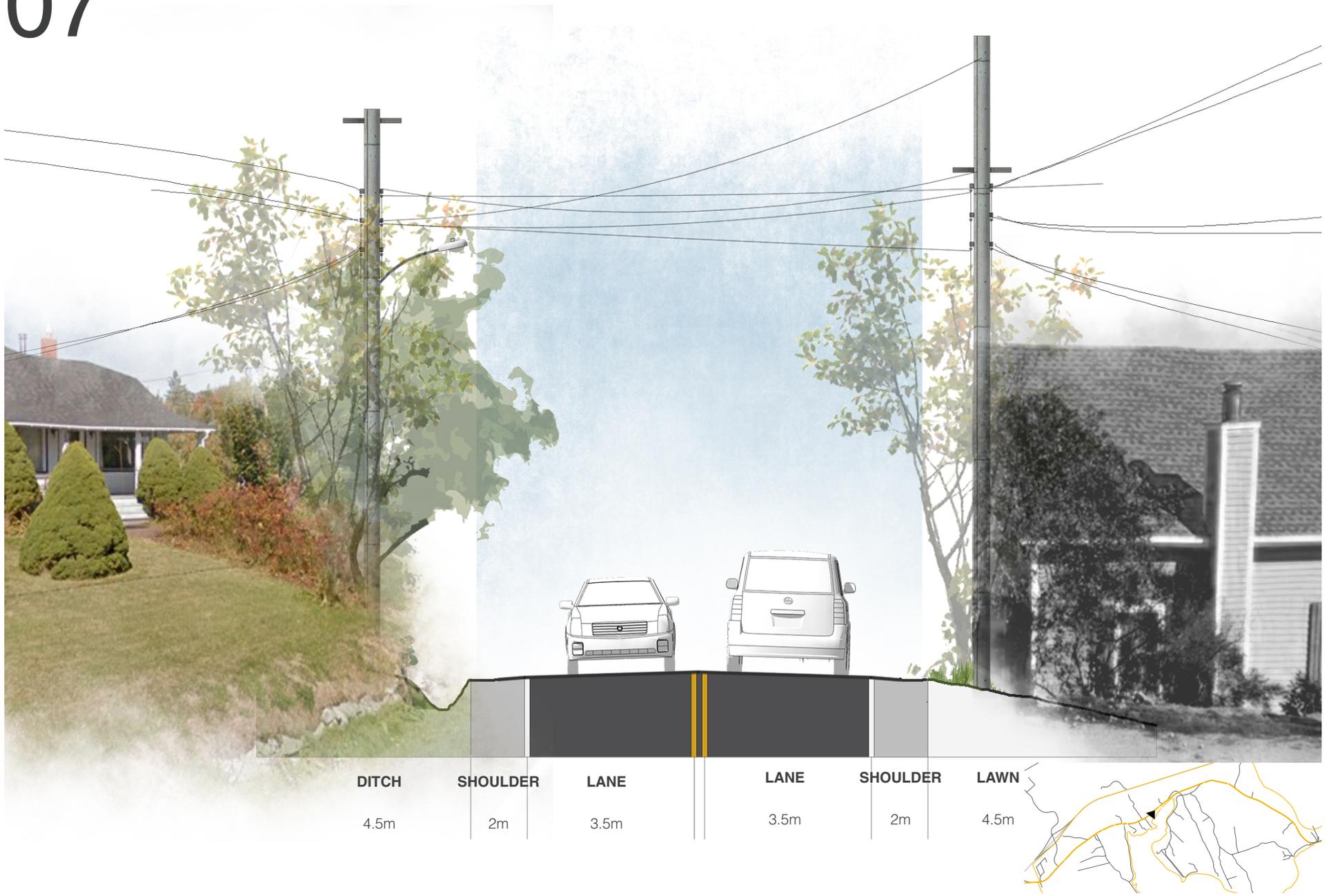
Sidewalk with grass verge

Curb is built into
with sidewalk

Sidewalk on one
side of the road



07



MAXIMUM
50

Utility Poles
& Overhead
Wires

Street Light

Utility Poles
& Overhead
Wires

Naturalized ditch

unpaved parking
area blends with the
shoulder of the road



08





Overhead
Utility Poles
& Wires

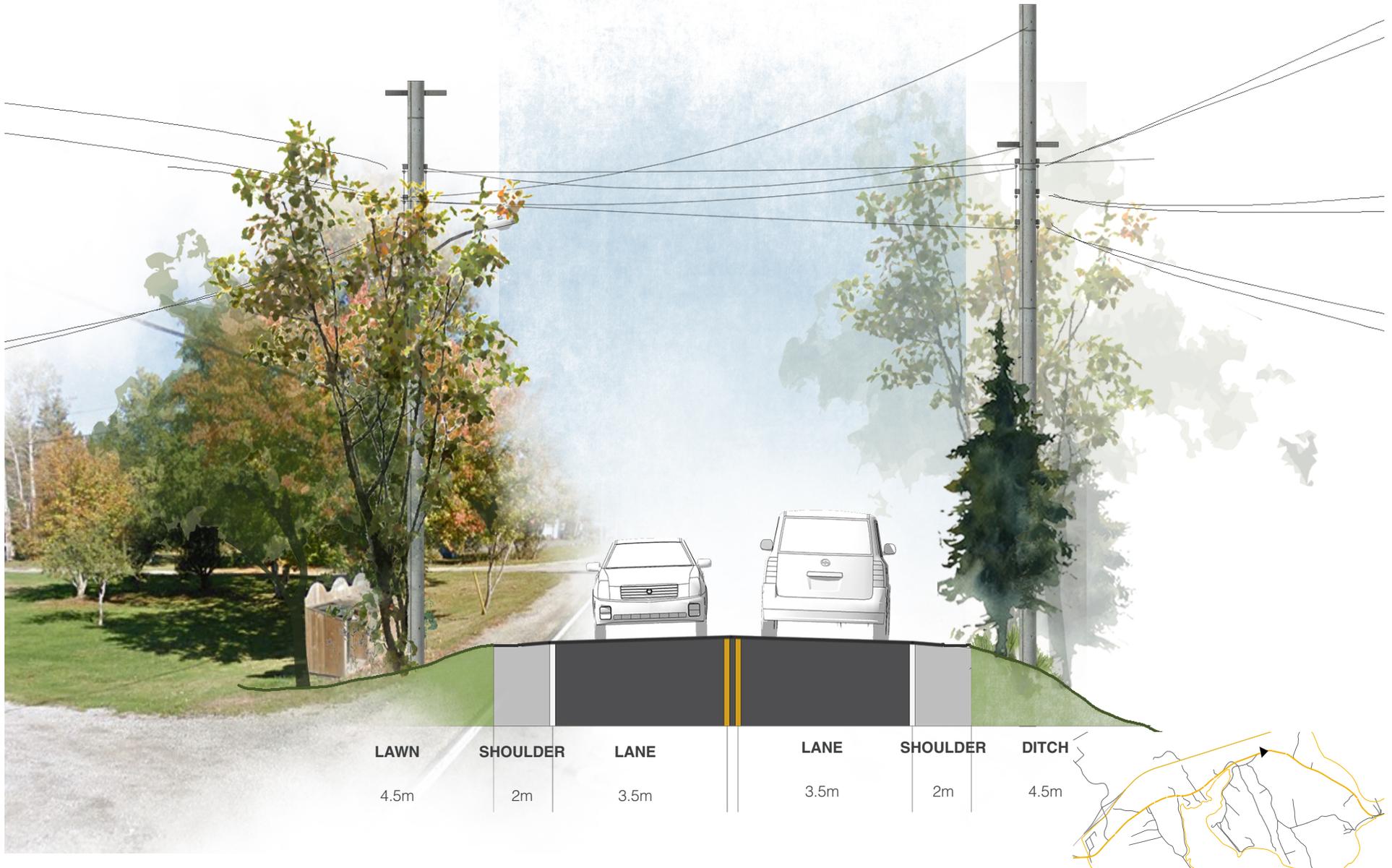
Utility Poles
& Overhead
Wires

Naturalized ditch

Road and
Traffic Signs



09



LAWN	SHOULDER	LANE	LANE	SHOULDER	DITCH
4.5m	2m	3.5m	3.5m	2m	4.5m

MAXIMUM
50



10



MAXIMUM

50

Utility Poles
& Overhead
Wires

Local
business signs

Street Light

Trash
receptacle

Sidewalk with grass verge

Curb is built into
with sidewalk

Sidewalk on one
side of the road

Naturalized ditch



11



MAXIMUM
70



Utility Poles
& Overhead
Wires

Road Signs

Naturalized ditch

Naturalized ditch



PART III

The Plan





9. PLAN PRINCIPLES

9.1 HOW TO READ THIS PLAN

This plan is rooted in the analysis of past community projects on the future of Hubbards, recent community feedback received during engagement activities within this Community Plan project, statistical and geographic data analyzed in earlier sections, identification of community sub-areas on the community development map and the inventory of streetscapes within the St. Margaret's Bay Road corridor. This plan outlines necessary steps and conceptual designs for the implementation of the earlier presented 'six big moves'.

Residents within the project area have an unmistakable desire to turn Hubbards into a community with a 'destination feel', which connects all the things that are great about

Hubbards in order to foster a vibrant community life. The plan's main objective is to put this vision—which stretches across two municipalities and involves a main street owned by the Province—into a coherent plan that outlines the first steps to kick off this transformation process.

This kind of step requires an out-of-the-box approach. With a community-driven project that affects the jurisdiction of three separate government authorities and two levels of government, it has proven problematic to create a strong vision for the redevelopment of the community while observing all government guidelines, especially as they relate to engineering standards adopted by road authorities. Readers

of this plan should therefore be aware that some proposals brought forward in this section are not necessarily approved by the government bodies having authority over roads. Nevertheless, it appears to be the best approach to move forward with a bold vision and to adapt that vision as it may be required, rather than starting off with the smallest common denominator that all levels of government can agree on.

It is therefore important to see the presented road upgrade solutions as conceptual ideas. Going forward, the proposed streetscape upgrades will have to go through several phases of design, from functional planning over detailed engineering design to construction drawings.

During these steps, the plans will inevitably be adapted and changed. It is therefore important to start off with a strong vision which creates momentum for the project, before road design details will be evaluated in advanced phases of design.

To give the reader a better understanding of conceptual design elements that may be currently at odds with provincial or municipal policy, the document will use a system indicating which plan elements can be readily implemented, or which proposals will—in some cases—require further discussions with applicable government authorities before they can be incorporated into final designs.



These symbols will indicate streetscape elements which are generally in line with current design practice.



These symbols will indicate streetscape elements which need further conversations with road authorities.

Figure 54: Indicators system used throughout this section



10. HUBBARDS LOOP + PLACEMAKING

10.1 INTRODUCTION

This section describes physical improvement proposals to the public realm of St. Margaret's Bay Road. First, overall good practices of main street design are discussed. These approaches include:

- + Intersection treatments
- + Filling in of underutilized corners
- + Crosswalks and medians
- + Access control
- + Placemaking

Then, proposed new cross sections of the road are suggested to replace some of the existing right-of-way cross sections. Finally, this is followed by a series of block-by-block conceptual site plans for the road.

For each block, two site plans are provided; an Existing Conditions site plan showing how St. Margaret's Bay Road is currently laid out, and a proposed site plan showing the changes that are suggested along the street.



10.2 INTERSECTION TREATMENT

In order to promote active modes of transportation and to create pedestrian-friendly realms, one of the key aspects is pedestrians safety. Pedestrians are the safest if they are fully separated from cars and trucks, e.g. when they are on a sidewalk that is fully separated from a street.

However, intersections are among the streetscape elements where the crossing of roadways by pedestrians is inevitable. Especially on a four-way intersection, a pedestrian walking in any direction must cross at least one roadway.

Where pedestrian road crossing are inevitable, they should be designed in a way that reduces the risk of collisions to a minimum. Minimizing the distance of road pavement that pedestrians must cross is a key strategy contributing to this target.

Consequently, intersections on pedestrian-oriented streets need 'bump-outs' that narrow down the roadway and increase the safe refuge area for pedestrians. These intersection treatments also have another function: they signal to drivers that this is an area with increased levels of activity, which is a visual cue to reduce speeds.

In addition to these benefits of tighter intersection roadways, the gained pedestrian space offers opportunities for small placemaking interventions. Even small arrangements of plantings or different sidewalk textures can add to the appeal of the streetscape and ultimately add to the success of a main street. There are limitations to this approach, however, as installations on street corners cannot impede sight lines and visibility for safety reasons.

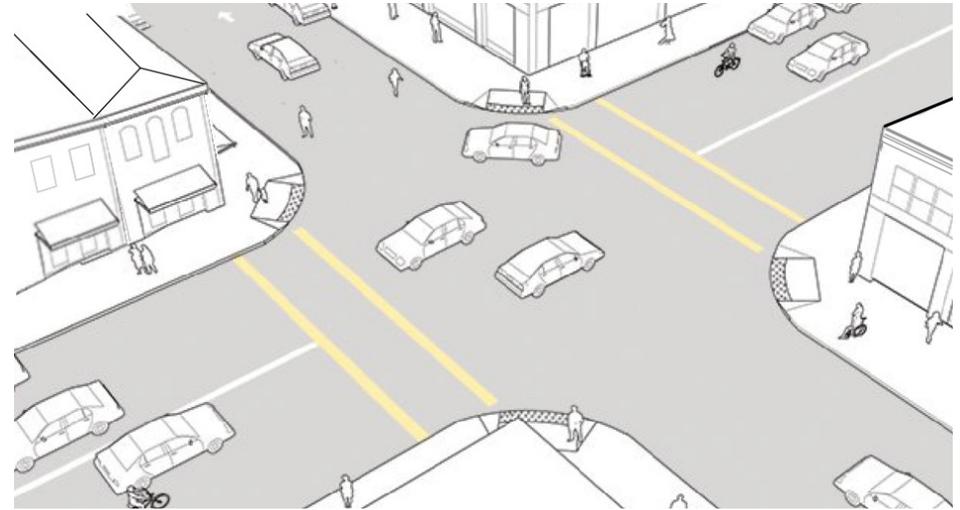


Figure 55: Existing Conditions (Image adapted from NACTO Urban Street Design Guide)

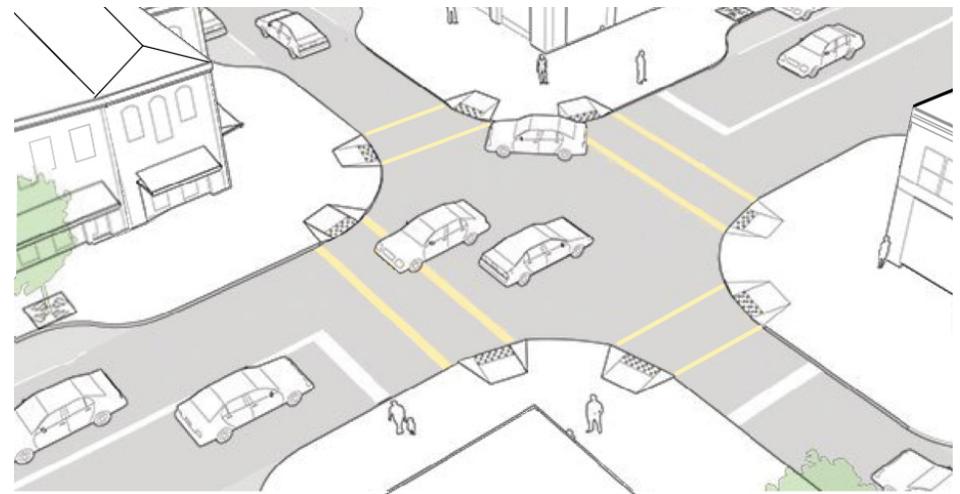


Figure 56: Preferred Approach (Image adapted from NACTO Urban Street Design Guide)



Pedestrian bump-outs are a common practice in some municipalities across Nova Scotia.

10.3 INTERSECTION REALIGNMENT

Related to the previous point, a redesign of intersections can open up significant amounts of land for better utilization. Street intersections in small Nova Scotian towns and rural areas are often designed around the priorities of vehicular traffic.

With no pedestrians in mind, large tracts of surface are often paved over to make corners of three- and four-way intersections seemingly easier to navigate. However, giving the drivers more choice how to navigate through an intersection can actually create more uncertainty on how to behave as a driver. Turning patterns become more difficult to predict for all traffic participants and can potentially result in dangerous situations.

Hubbards features several of these oversized intersections. In some cases, it is not entirely clear why they are shaped in their current form.

For instance, Shankel Road meets St. Margeret's Bay Road in a very sharp angle, which results in limited visibility of traffic travelling eastbound to Queensland. While this design is already problematic, the difficulty of this intersection is exacerbated by the fact that the intersection is adjoining

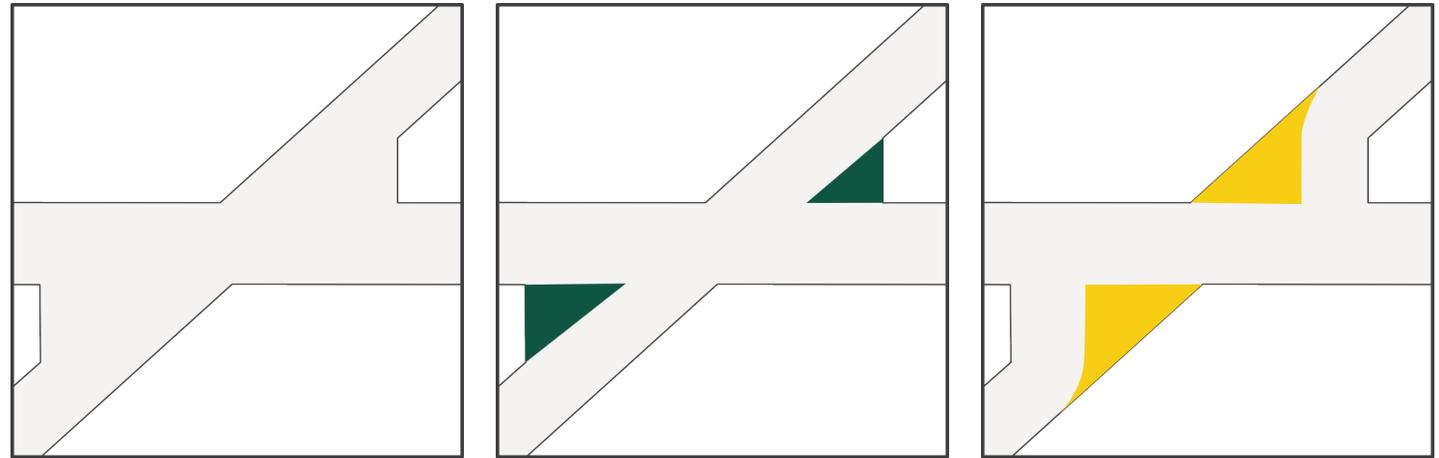


Figure 57: Various approaches to intersection design. Green and Yellow polygons show options to better define traffic routing.

two driveways and a crosswalk leading to the local elementary school.

There is sufficient provincially owned right-of-way in that area, allowing for a gentle curve of the local Shankel Road before its intersection with St. Margeret's Bay Road.

Such an approach would eliminate the dangerous angle, relocate the intersection away from the school's crosswalk and provide for a 90-degree intersection with better visibility. It is a typical road realignment strategy applied in such cases.

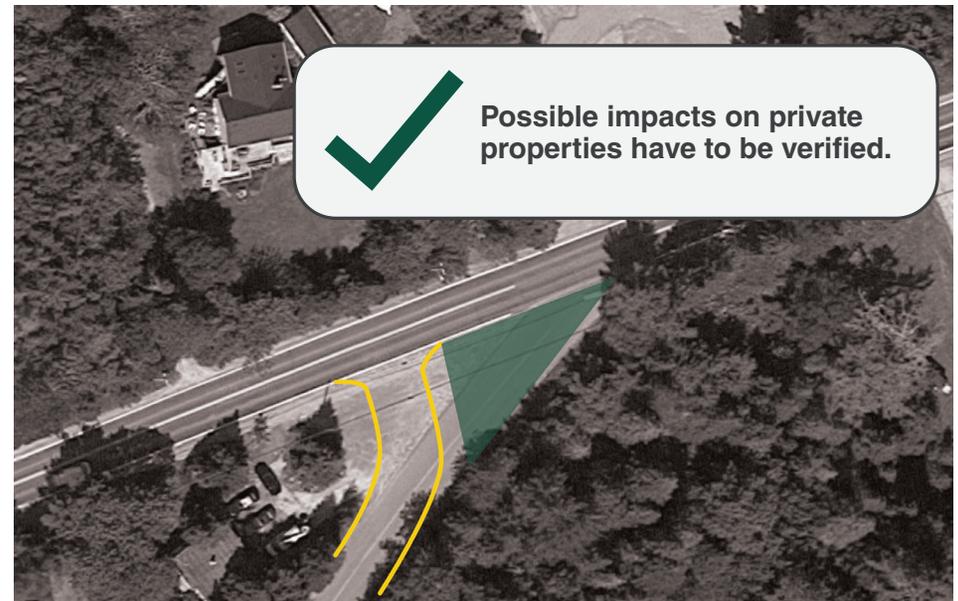


Figure 58: Concept for road realignment at eastern end of Shankel Road.

10.4 ACCESS MANAGEMENT

Access control techniques are recommended. These include managing driveway location, spacing, and design, using aesthetic treatments such as landscaped features, pavement textures and designs, removing and consolidating obtrusive signs, and adding uniform street lighting along main streets such as St. Margaret's Bay Road.

As the road owner, the provincial Department of Public Works should consider these access management techniques to promote safe and efficient travel for all road users and to enhance the aesthetics of transportation corridors in the core area of the community.

In some locations of the road, driveways are particularly large and tend to blend with parking arrangements in front of businesses. In these areas it is also recommended that planting beds be installed to improve access control. This will limit locations where cars can turn off the road to enter a parking area, reduce the number of hazard points for accidents between cars and pedestrians, and also provide a visual break between the road and the adjacent properties.

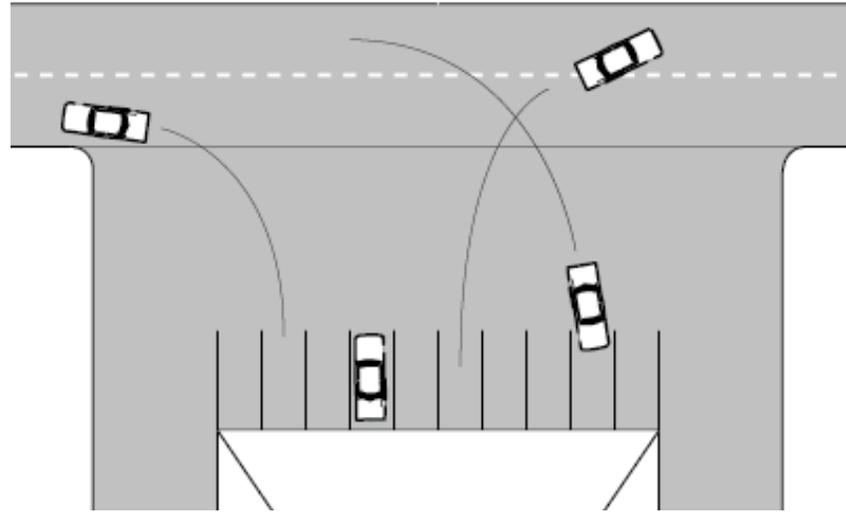
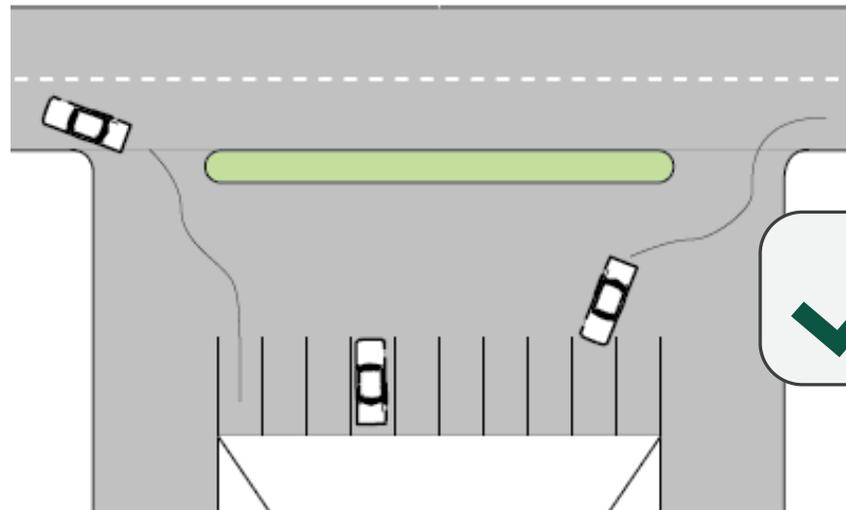


Figure 59: Existing access management conditions in front of many properties.



Access management exercise to be done in consultation with landowner.

Figure 60: Recommended access management arrangements.

10.5 CROSSWALKS AND MEDIANS

Crosswalk medians and pedestrian safety islands are specific streetscape design elements increasing pedestrian safety at crosswalks. Similarly to pedestrian sidewalk bump-outs at intersections, they reduce the crossing distance during which pedestrians are directly exposed to traffic.

By creating a safety zone between the two lanes of travel, pedestrians only need to watch out for direction of travel at a time. Medians decrease the risk of random turns from adjacent driveways or intersections, creating a clear and predictable flow of traffic within the area where they are applied.

Ideally such pedestrian median islands should have a width of about 3 metres, which creates a reasonable mid-road buffer for pedestrians and allows design engineers to alternate pedestrian islands with left turning lanes on the road. However, where the right-of-way does not have the width to accommodate additional lanes of traffic, slimmer islands can be utilized and are still better than no islands at all.

Central medians or safety islands work just as well for multi-use path crossings of roads, e.g. in locations where the Rum Runners Trail crosses St. Margeret's Bay Road.

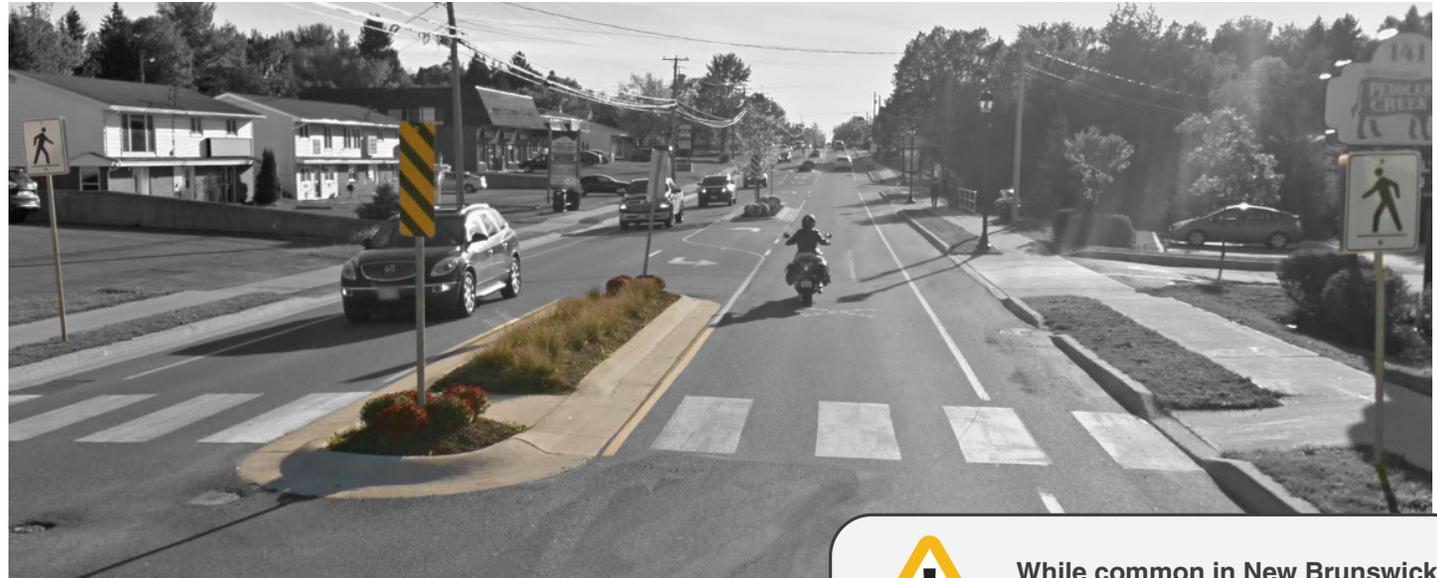


Figure 61: Example of a paved crosswalk median in Rothesay, NB.



While common in New Brunswick and other parts of Canada, medians are currently under review in NS.



Figure 62: Raised crosswalk median in Naples, ME.



Figure 63: Painted crosswalk median in Austin, TX.

10.6 PLACEMAKING

The upgrade of a streetscape would not be complete without amenities for pedestrian road users. Safety is an important precondition for walking, but on its own it does not accomplish a truly pedestrian environment.

To create that vibrant community core which is much desired by the residents of Hubbards, streetscape elements containing art and promoting social interaction need to be incorporated into the streetscape.

✓ Placemaking typically occurs next to the road and is generally not problematic.



“To be favored, a walk has to satisfy four main conditions: it must be useful, safe, comfortable, and interesting.”
– Jeff Speck

Natural elements within the streetscape improve the visual appearance of a street and can also help to improve stormwater management by natural retention of rain water.

Seating opportunities should be arranged so that they increase the likelihood of conversations and interaction. They also fulfill an important accessibility aspect: older adults are numerous in our aging population, and frequent opportunities to rest increase their ability to walk further distances during their daily errands.



Figure 64: Placemaking elements of recent streetscape upgrades in the Maritimes: Shediac, NB (top) and Port Hawkesbury, NS (bottom).

10.7 PROPOSED CROSS SECTIONS

DESIGN APPROACH

Based on the active transportation needs analysis and the 'Six Big Moves' derived therefrom, this plan advances a proposal of new five new right-of-way configurations of St. Margaret's Bay Road between the Rum Runners Trail head to the west and the J.D. Shatford Elementary school to the east.

The new cross sections types are numbered and followed by the letter 'N', which indicates 'new'. The cross-sections at the Rum Runners trailhead and elementary school are identical, and propose the same type of gateway feature that will mark the beginning and the end of the driver's passage through the central stretch of the community.

While right-of-way configurations between trail head and the school could be replaced by the suggestions from the following pages, the cross sections are expected to remain as shown in the earlier analysis outside of these extents.

This approach results in the following five proposed types of cross sections for St. Margaret's Bay Road.

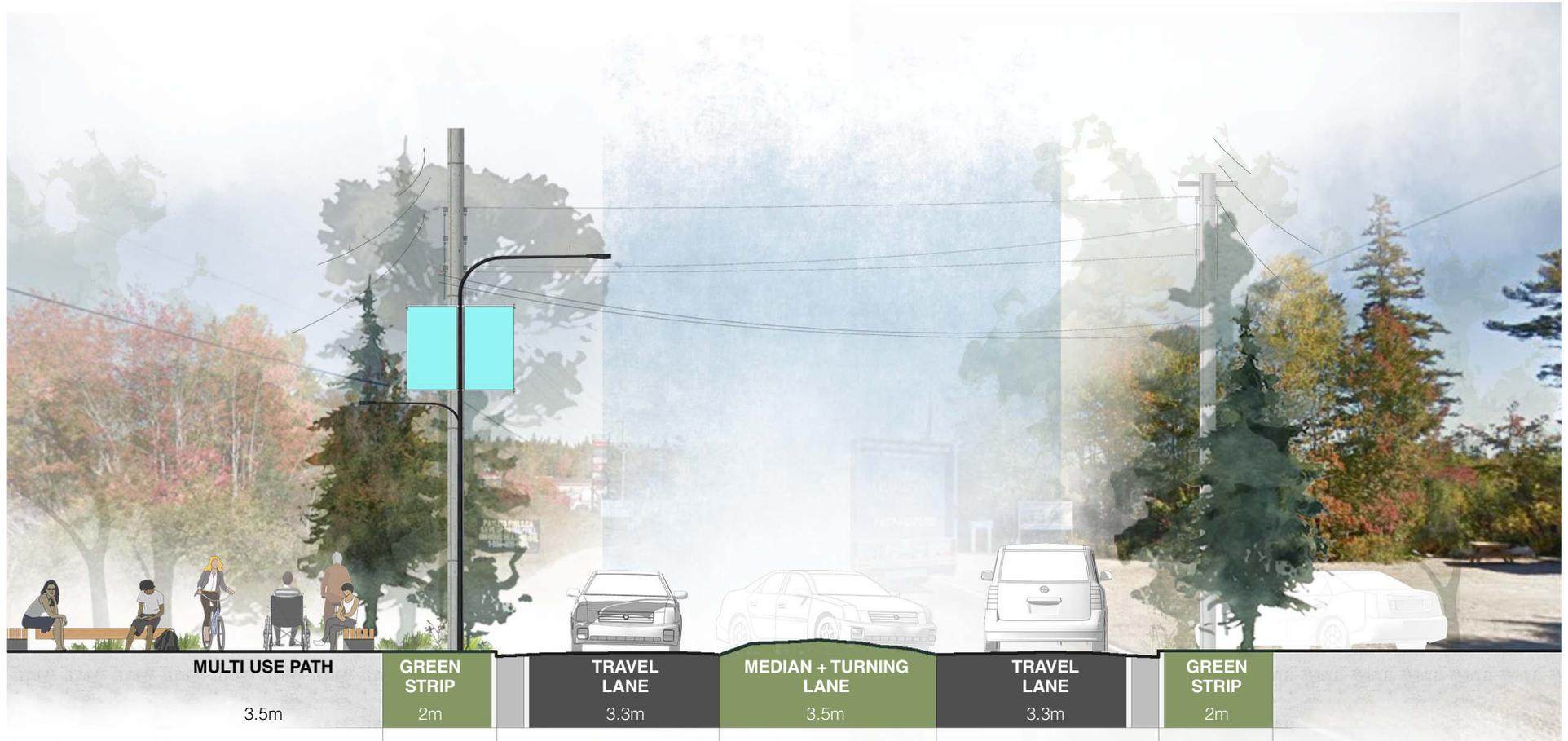


Figure 65: Overview of proposed, new street cross-section types

01 N

Rum Runners Trailhead to Route 329

A new multi-use path is proposed to begin at a junction with the existing Rum Runners Trail, and to continue east on the northern side of St. Margaret's Bay Road. Being a gateway to the community, this section will accommodate a crosswalk median island between the two lanes of travel, which will also double as a turning lane for vehicles turning to the trail head parking lot.



02N

Route 329 to Fox Point Front Road

Throughout this stretch of the road, the multi-use path on the northern side of the road is joined by the existing 1.5 metre-wide sidewalk on the southern side.



03N

Fox Point Front Road to Post Office

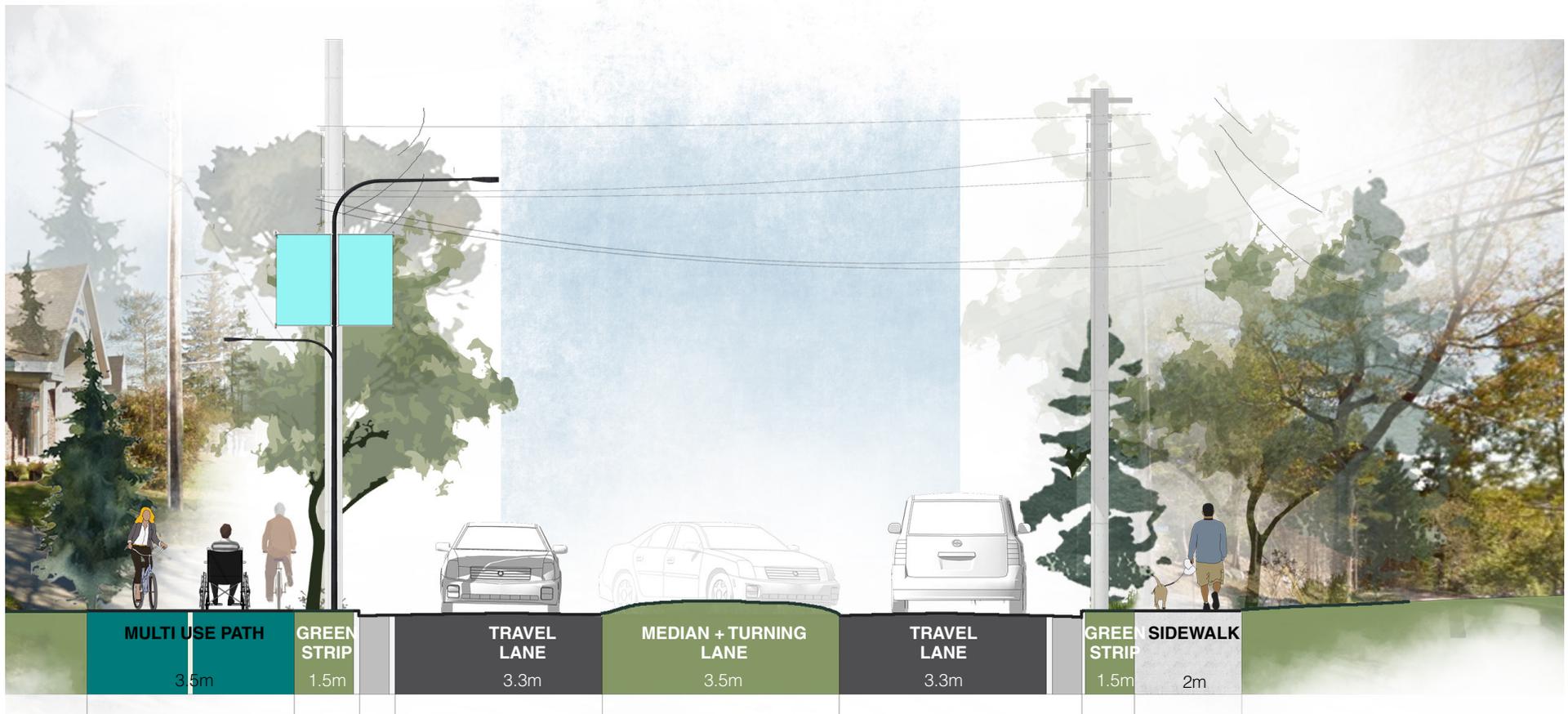
This section is similar to the previous one, except that instead of the existing sidewalk between Route 329 and Fox Point Front Road, this section includes newly proposed 2 metre-wide sidewalk on the southern side of the road. On the northern side, the multi-use path is maintained and continues uninterrupted. Green strips between the edges of pavements and proposed active transportation features measure 1.5 metres and allow for some limited growth of vegetation, which increases the comfort of pedestrians through visual separation. At the same time, the increased 'side friction' of the plantings may help to reduce travel speeds on the road.



04N

Post Office to Community Waterfront

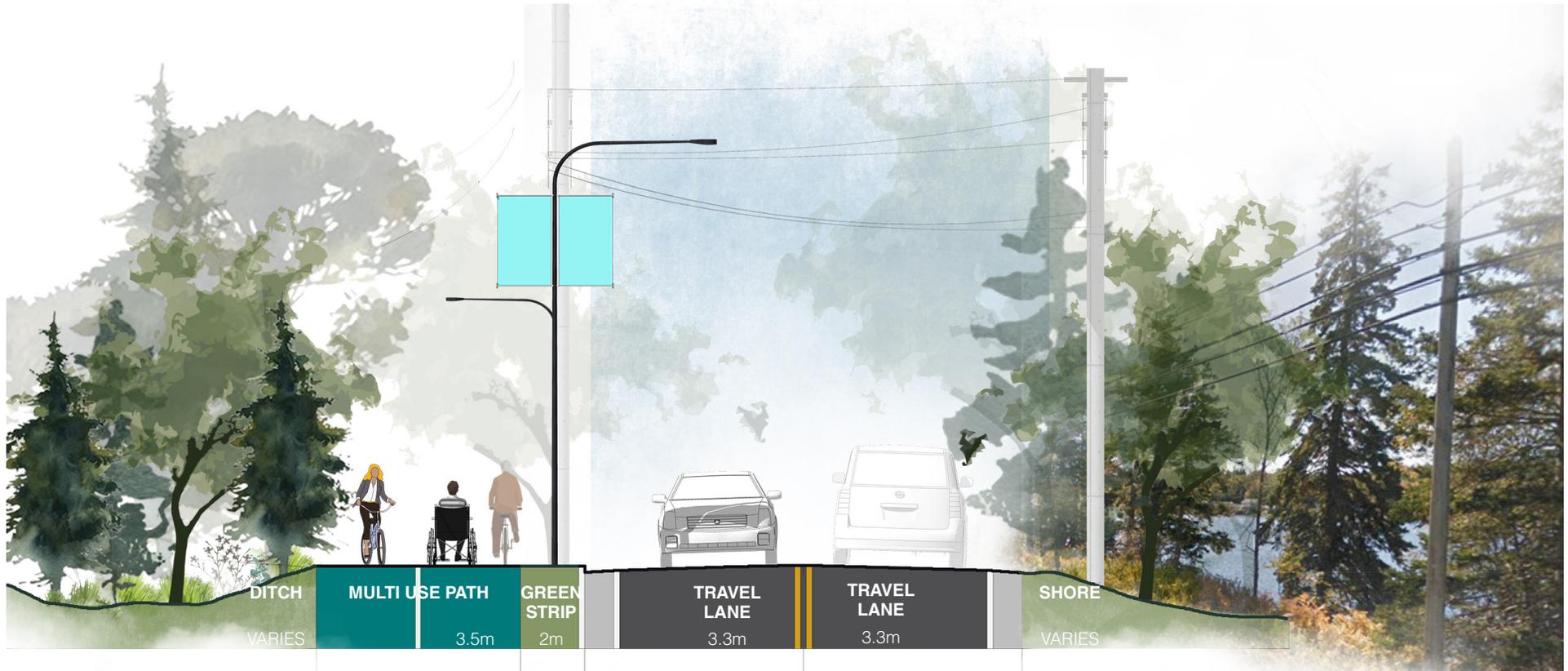
This is the busiest section of the community core. Apart from the two continued active transportation features on the northern and southern side of the road, this section is also suggested to accommodate median islands for increased pedestrian safety. Again, the median islands will alternate with left-turn lanes.



05N

Waterfront Amphitheatre to School

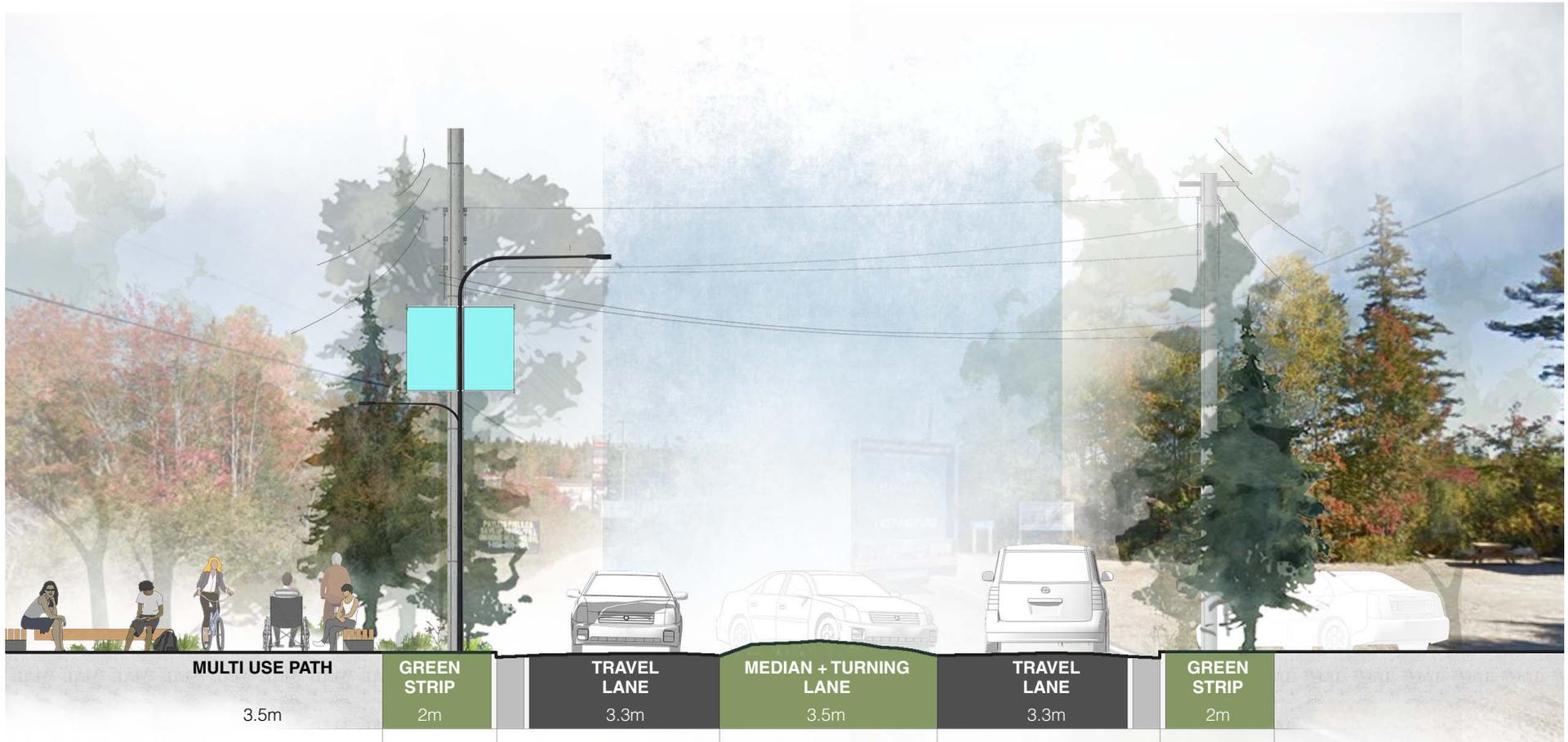
Travelling further east, downhill past the community waterfront access road, the character of road's surroundings change significantly. The area transitions abruptly into a residential environment without commercial properties. As a consequence of this, there is no need to continue the sidewalk on the southern side of the road, and the multi-use path on the northern side will cover the active transportation needs of the community in this area.



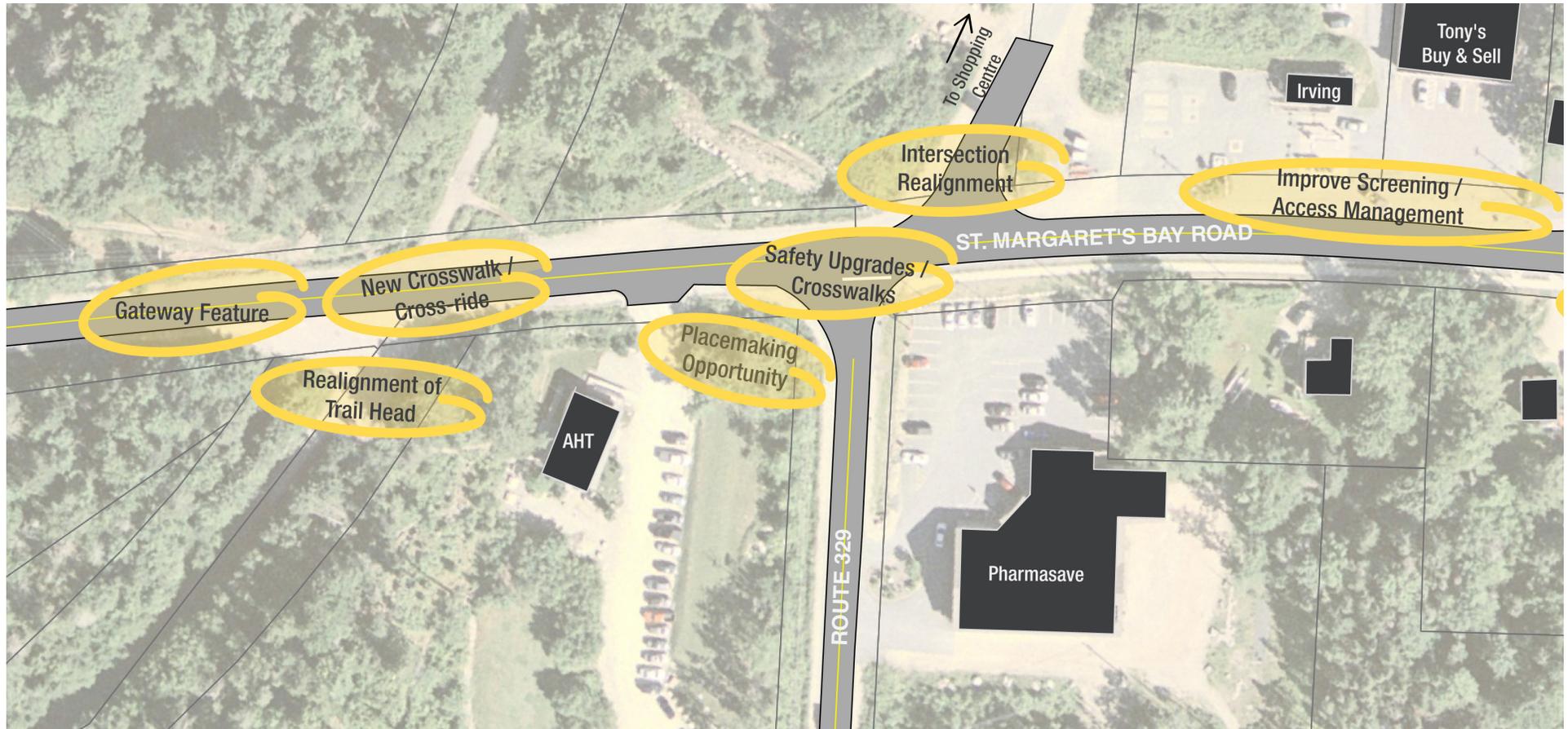
01N (repeat)

Shatford Elementary School and Shankel Rd

At the end of St. Margaret's Bay Road's extent of upgrades, the gateway cross-section type is repeated once more. The multi-use path leads to the trail connector behind the school and ends there. The intersection with Shankel Road is realigned, resulting in another three-lane road configuration, featuring median islands alternating with turning lanes repeatedly.



10.8 TRAIL HEAD TO GAS STATION



EXISTING CONDITIONS

When motorists enter the community travelling east, they notice a change in scenery after crossing the Rum Runners Trail. The landscape suddenly opens up and various commercial developments become visible. However, it would be useful if drivers would already receive such visual cues before arriving at the trail.

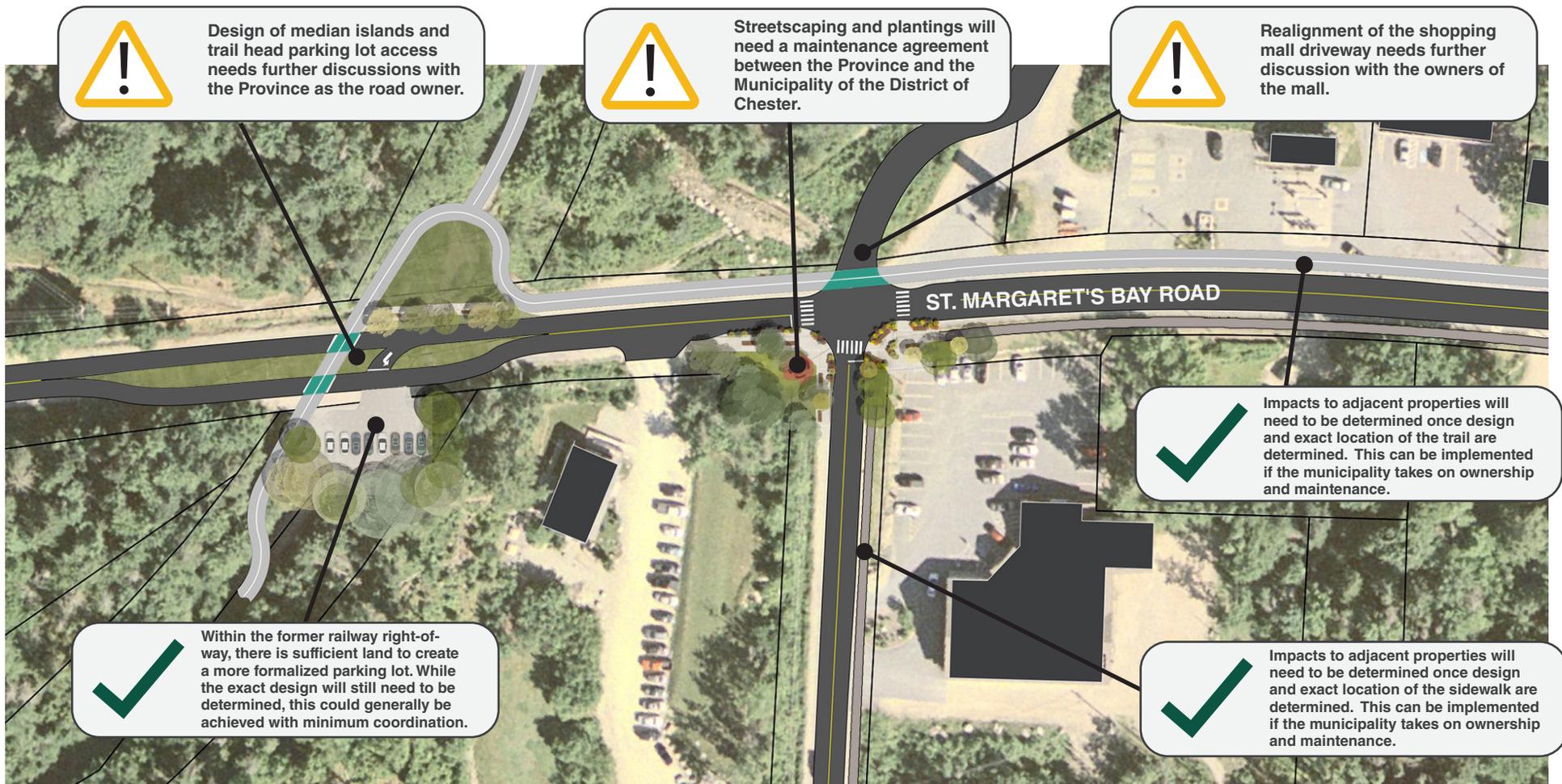
Also, the combination of a trail crossing and trailhead parking lot is problematic for the safety of all traffic participants, as vehicles pulling in and out of the lot can collide with trail users crossing the road.

Heading east, there are two slightly offset intersections right behind

each other: one with Route 329 and the other with the driveway to the Hubbards Shopping Centre. The offset tends to cause dangerous driving maneuvers, and the edges of the intersections are poorly defined, thus creating a grey area between road surface and shoulders.

The corner of the Aspotogan Heritage Trust property does currently accommodate a community gardening project, which could be expanded to a corner park in that location.

The area in front of the gas station has about 110 metres of unmanaged road frontage.

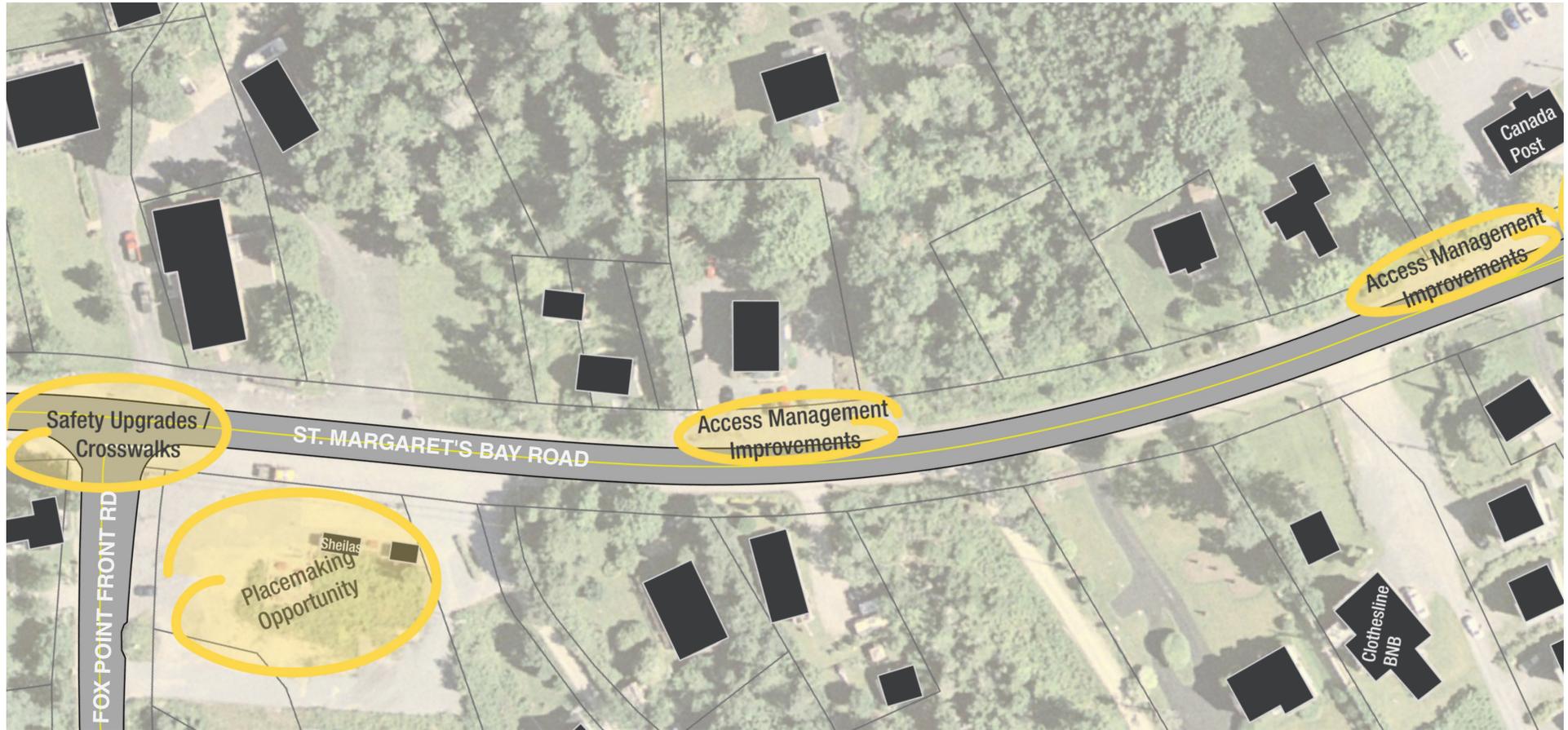


PROPOSED IMPROVEMENTS

The proposed improvements for this section of St. Margaret's Bay Road include:

- + realignment of the Rum Runners Trail at the crossing of St. Margaret's Bay Road to allow for a separation of the crosswalk and parking lot driveway
- + Increase of the trailhead parking lot size to address capacity issues and spillover of cars into the shoulder
- + Pedestrian/bicyclist safety island at crossing to increase safety of trail users
- + Dedicated left-turn lane for trailhead parking lot
- + Multi-use path on northern side of the road leading east
- + Realigned Route 329 intersection with pedestrian safety bump-outs and placemaking elements on street corners
- + Three new crosswalks and one crosswalk/crossride at intersection
- + Dedicated access driveways at gas station and Buy&Sell Shop
- + New sidewalk along Route 329 towards the Sailing Club

10.9 FOX POINT FRONT ROAD TO POST OFFICE



EXISTING CONDITIONS

The St. Margaret's Bay Roads intersection with Fox Point Front Road has very large and loosely defined shoulders. This circumstance is exacerbated by the fact that the former gas station property in the southeastern corner is one large patch of gravel without curbs or defined edges.

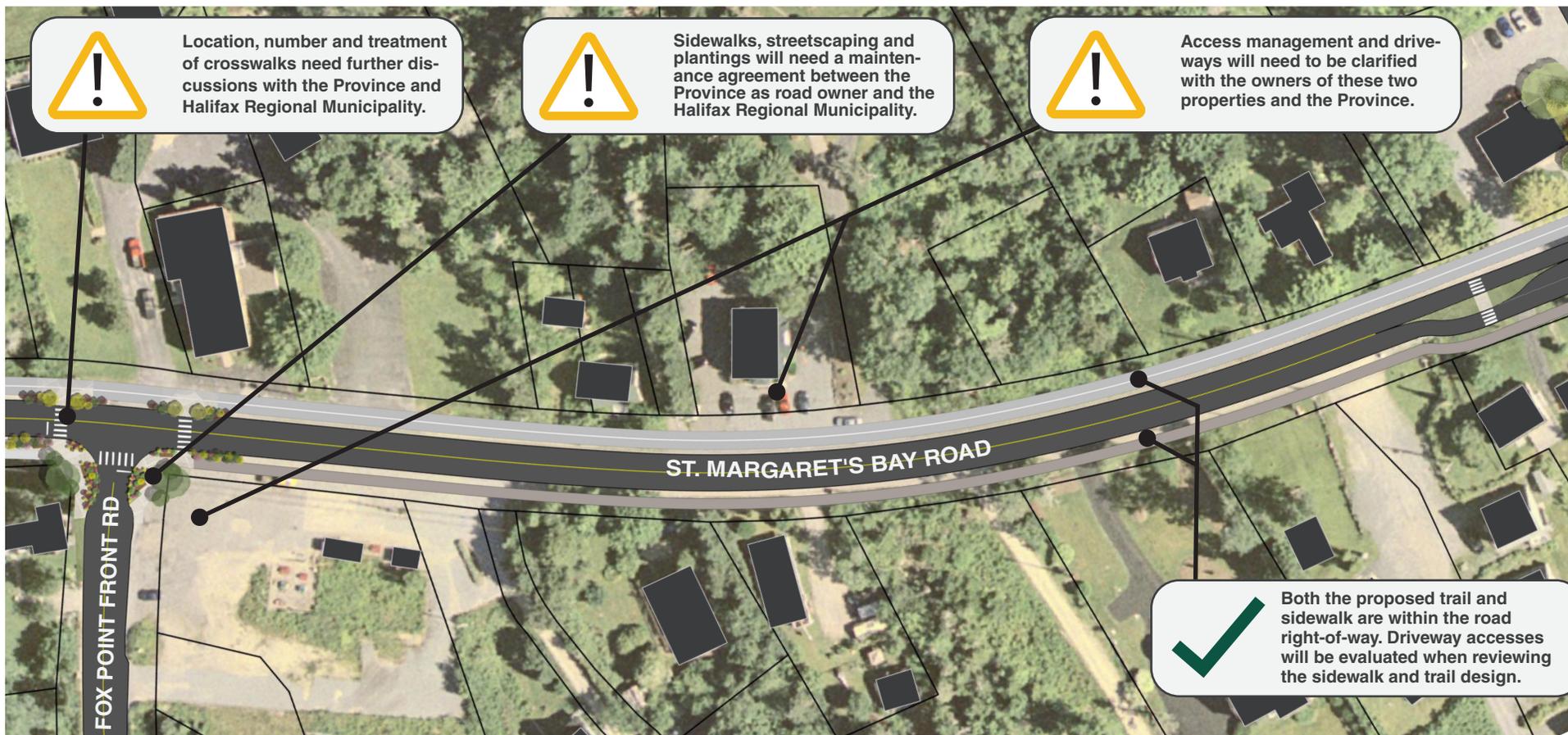
The property across from this location and other properties further east on the road could benefit from better definition of access points as well.

The only municipal sidewalk in Hubbards, which starts at Route 329, ends on the southwestern corner of the Fox Point Front Road intersection

and leaves pedestrians without a clear path to continue their walk.

In front of the Post Office there is an informal parking lot in the provincial highway right-of-way, where cars park perpendicular to the road. Since this area is not a part of the property and there is ample parking in the

rear of the building, this dangerous parking arrangement is redundant and can likely be adjusted.

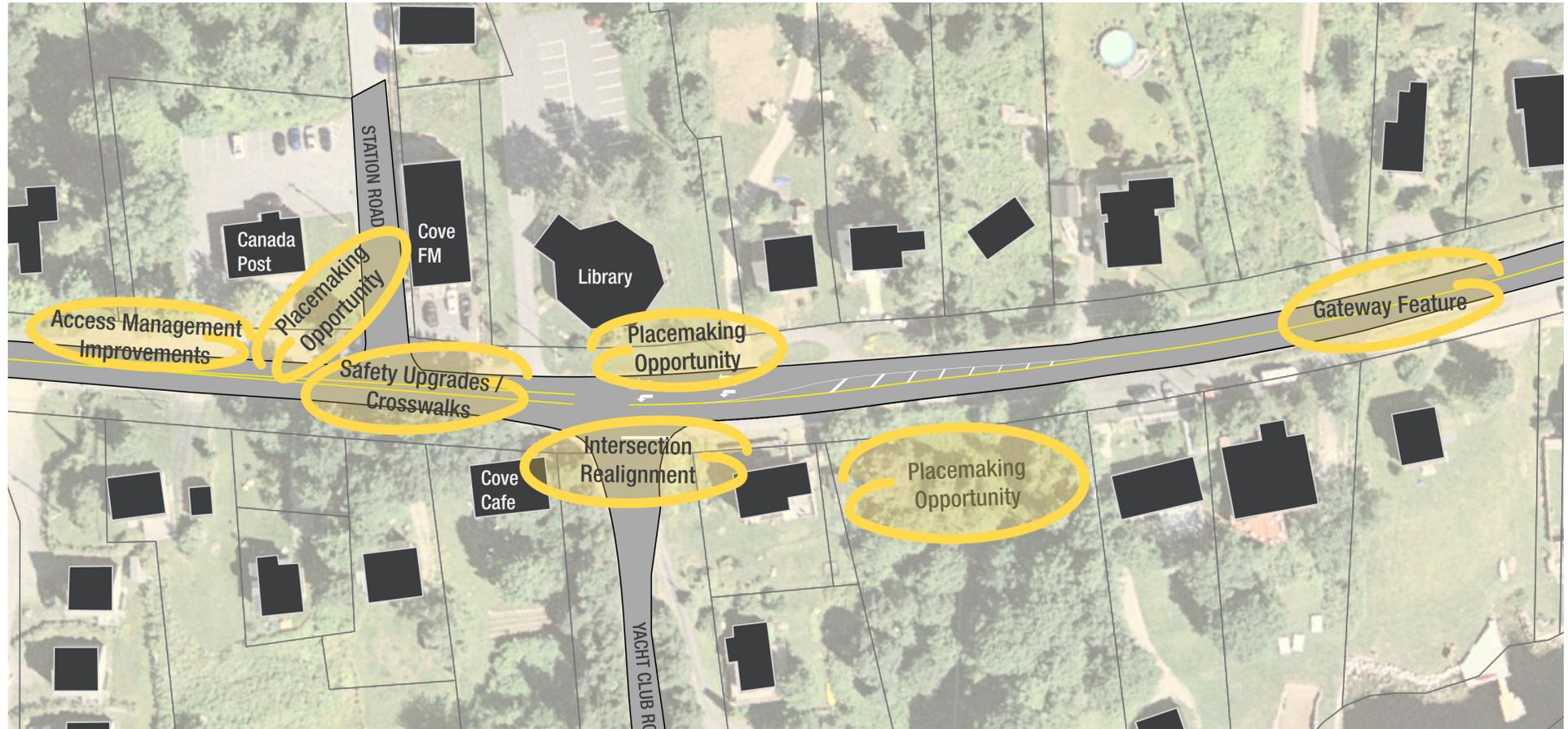


PROPOSED IMPROVEMENTS

The proposed improvements for this section of St. Margaret's Bay Road include:

- + Tightening of road surfaces at the intersection of St. Margaret's Bay Road and Fox Point Front Road
- + Landscaping and stormwater retention elements in the resulting pedestrian bump-outs
- + Continuation of the multi-use path along the northern side of the road, coupled with access management measures at all properties with loosely defined driveways
- + New crosswalk in front of post office building
- + pedestrian safety island supporting the functionality of that crosswalk
- + new sidewalk on southern side of St. Margaret's Bay Road, in continuation of the existing sidewalk

10.10 POST OFFICE TO WATERFRONT



EXISTING CONDITIONS

This is one of the busiest sections of St. Margaret's Bay Road. There is a group of landmark buildings enclosing the Station Road and Yacht Club Road intersections from all sides, creating a feeling of a central community plaza. This circumstance is inviting placemaking opportunities at the corner of Station Road and in front of

the library. At the latter location, this is already occurring to a limited extent.

The alignment of roads and driveways in this area creates various hazards for road users. The frequency and proximity of exit and entrance points to St. Margaret's Bay Road both create possibilities to turn in this area.

Pedestrians crossing the road in this vicinity have to be very aware of all the possible directions where cars can come from. This is all aggravated by the fact that the intersection with Yacht Club is situated on the location of a hill crest, which further reduces visibilities.

Further east, the lands of the Community Waterfront Association front on St. Margaret's Bay Road. Looking from the road, only a mowed patch of grass and a driveway leading to the water are visible. It is not instantly obvious that this is a property for public enjoyment.



This is a conceptual drawing of how pedestrian safety principles could be implemented. The entire intersection will need detailed design and is subject to change.



Impacts to adjacent properties will need to be determined once design and exact location of the trail are determined. This can be implemented if the municipality takes on ownership and maintenance.

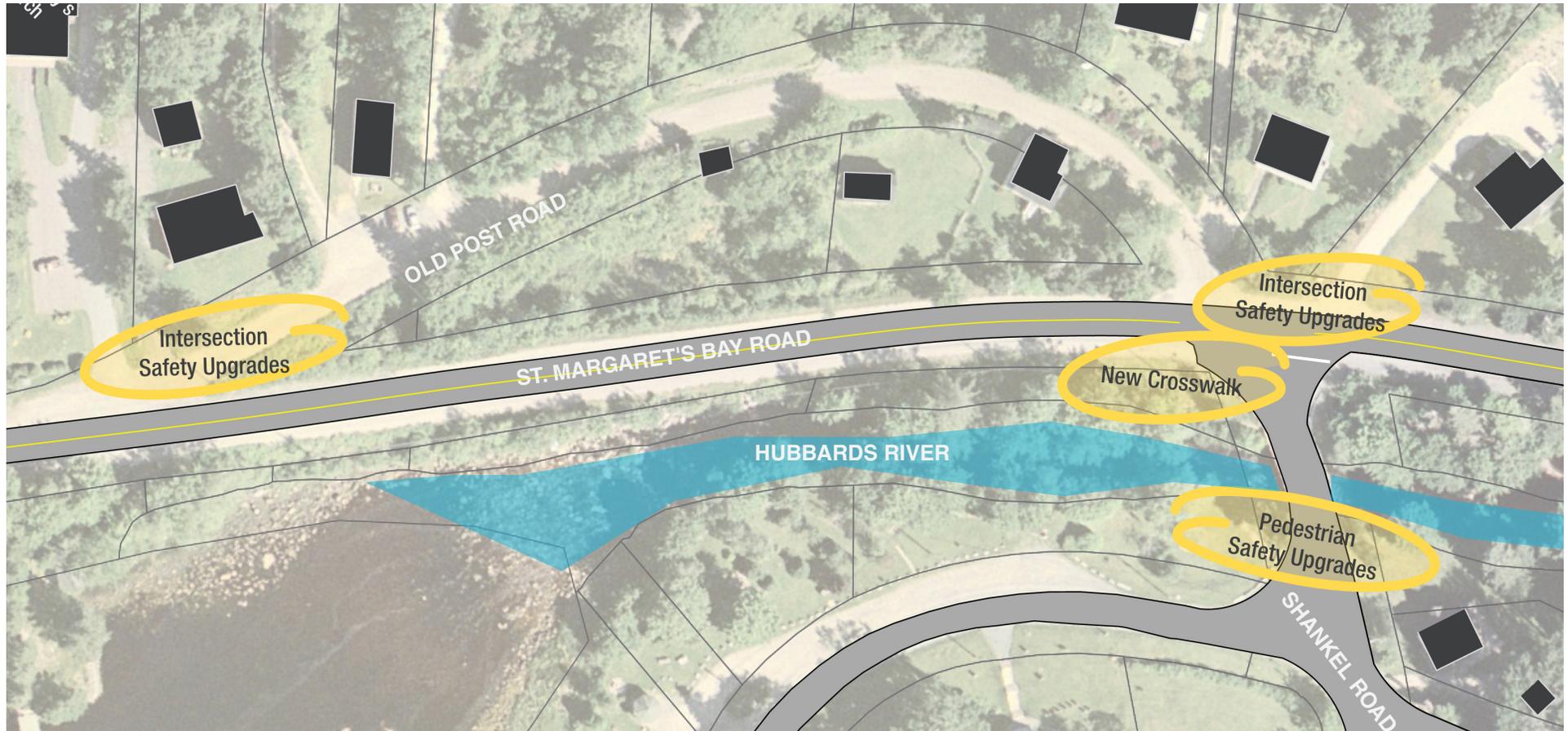


PROPOSED IMPROVEMENTS

The proposed improvements for this section of St. Margaret's Bay Road include:

- + Widening of St. Margaret's Bay Road to include the width of a third travel lane, which is used to alternate between pedestrian safety islands and dedicated left-turn lanes to various roads and driveways.
- + Creation of two new crosswalks on both sides of Station Road; with both crossings being supported by safety islands
- + New crosswalk across Yacht Club Road, supported by pedestrian safety bump-outs on both street corners
- + Extension of the new sidewalk to the top of the community waterfront association property
- + New community square at the top of the community waterfront property, including stands for food trucks or permanent sheds that can be rented to vendors
- + Placemaking on library and post office properties
- + Landscaping and stormwater retention features on the corners of Yacht Club Road

10.11 OLD POST ROAD TO SHANKEL ROAD



EXISTING CONDITIONS

A closer look at the routing and alignment of roads in this area reveals how the road network used to look in the past. Instead of the straightened St. Margaret's Bay Road, the former main road of Hubbards was following the course of Old Post Road, before it curved and crossed the Hubbards River in the location of today's Shankel

Road bridge over that watercourse. From there the main road followed the course of today's Shankel Road, until reached St. Margaret's Bay Road in front of the elementary school.

This history of the road network has left a legacy of awkward intersections in the area, which affect the traffic flow

and safety to this day. On its western end, Old Post Road merges onto St. Margaret's Bay Road in a sharp angle with limited visibilities and a wide area to be crossed by pedestrians.

On both sides of the bridge crossing Hubbards River, there are irregular forms of intersections with multiple

options to take turns. In addition that, the bridge itself features a surface of wood planks without any markings, organization of traffic lanes or pedestrian areas.

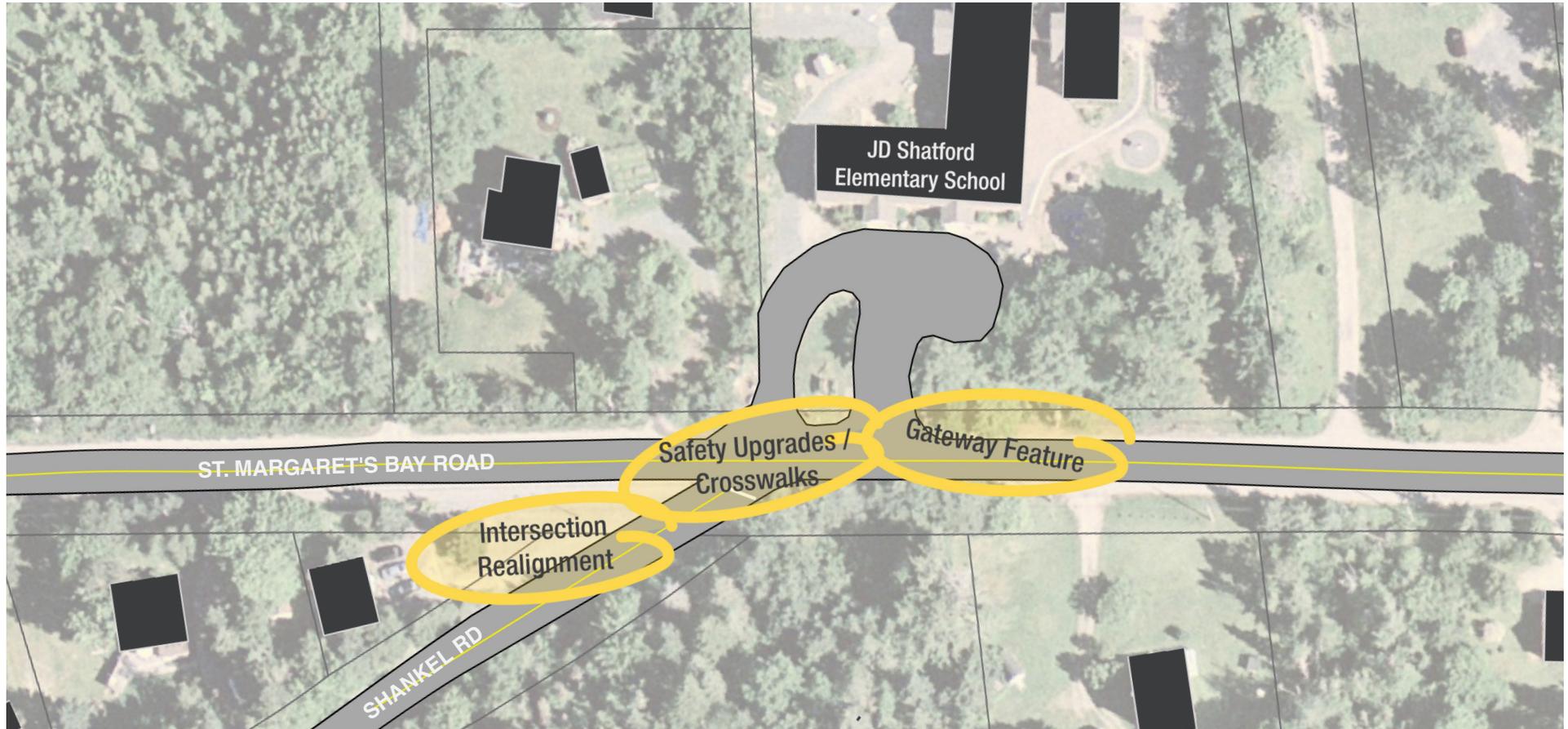


PROPOSED IMPROVEMENTS

The proposed improvements for this section of St. Margaret's Bay Road include:

- + Realignment of the western Old Post Road intersection, creating a 90 degree angle
- + Closure of the exit from Old Post Road onto St. Margaret's Bay Road
- + Realignment of intersection to allow for direct crossing from Shankel Road into one-way entrance to Old Post Road
- + Creation of safety buffers on both street corners of Shankel Road, creating opportunities for landscaping and pedestrian space
- + New crosswalk on the same corner
- + Creation of raised sidewalk/boardwalk for pedestrians across the bridge, merging into Bishop's Park
- + Continuation of multi-use path on northern side of the road

10.12 SHANKEL ROAD INTERSECTION AT ELEMENTARY SCHOOL



EXISTING CONDITIONS

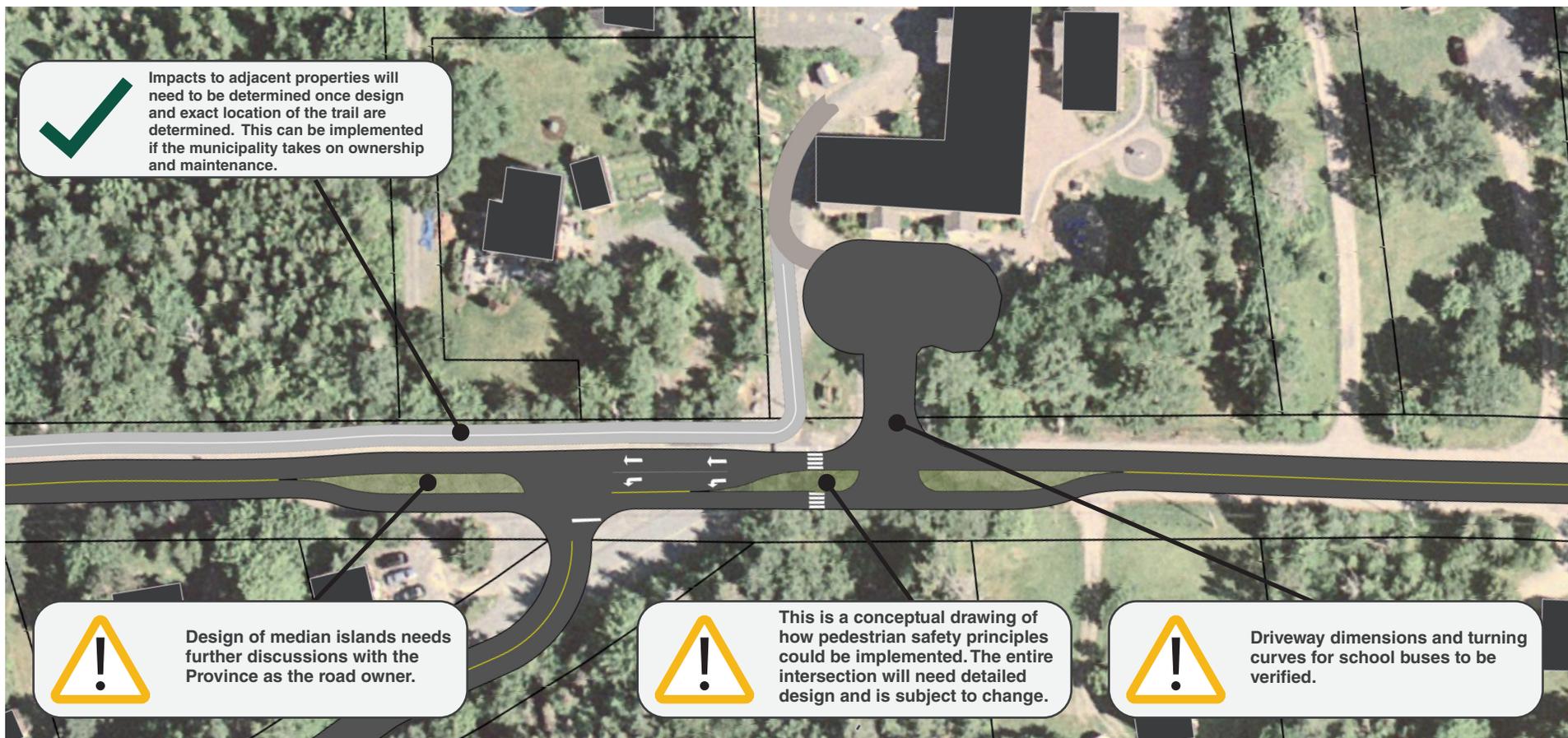
The area in front of the elementary school is another street design element which was left behind by the legacy of the historic highway routing. Shankel Road—being the old highway up until the late 19th century—merges on to St. Margaret's Bay Road at a very sharp angle.

While this is already a problematic three-way intersection by itself, there are two additional factors that decrease the level of safety in this location: two driveways leading in and out of the school parking as well as a crosswalk, used to a large extent by schoolchildren.

If the old garage property on the corner of both roads is ever to be reactivated for some commercial uses, the dangers of this intersection would be heightened.

The school property also has a newly established connector trail access to the Rum Runners Trail at the back of

the property. Walkers, runners and cyclists access that connector by using the western school driveway, and passing the school building along the western property boundary.



✓ Impacts to adjacent properties will need to be determined once design and exact location of the trail are determined. This can be implemented if the municipality takes on ownership and maintenance.

⚠ Design of median islands needs further discussions with the Province as the road owner.

⚠ This is a conceptual drawing of how pedestrian safety principles could be implemented. The entire intersection will need detailed design and is subject to change.

⚠ Driveway dimensions and turning curves for school buses to be verified.

PROPOSED IMPROVEMENTS

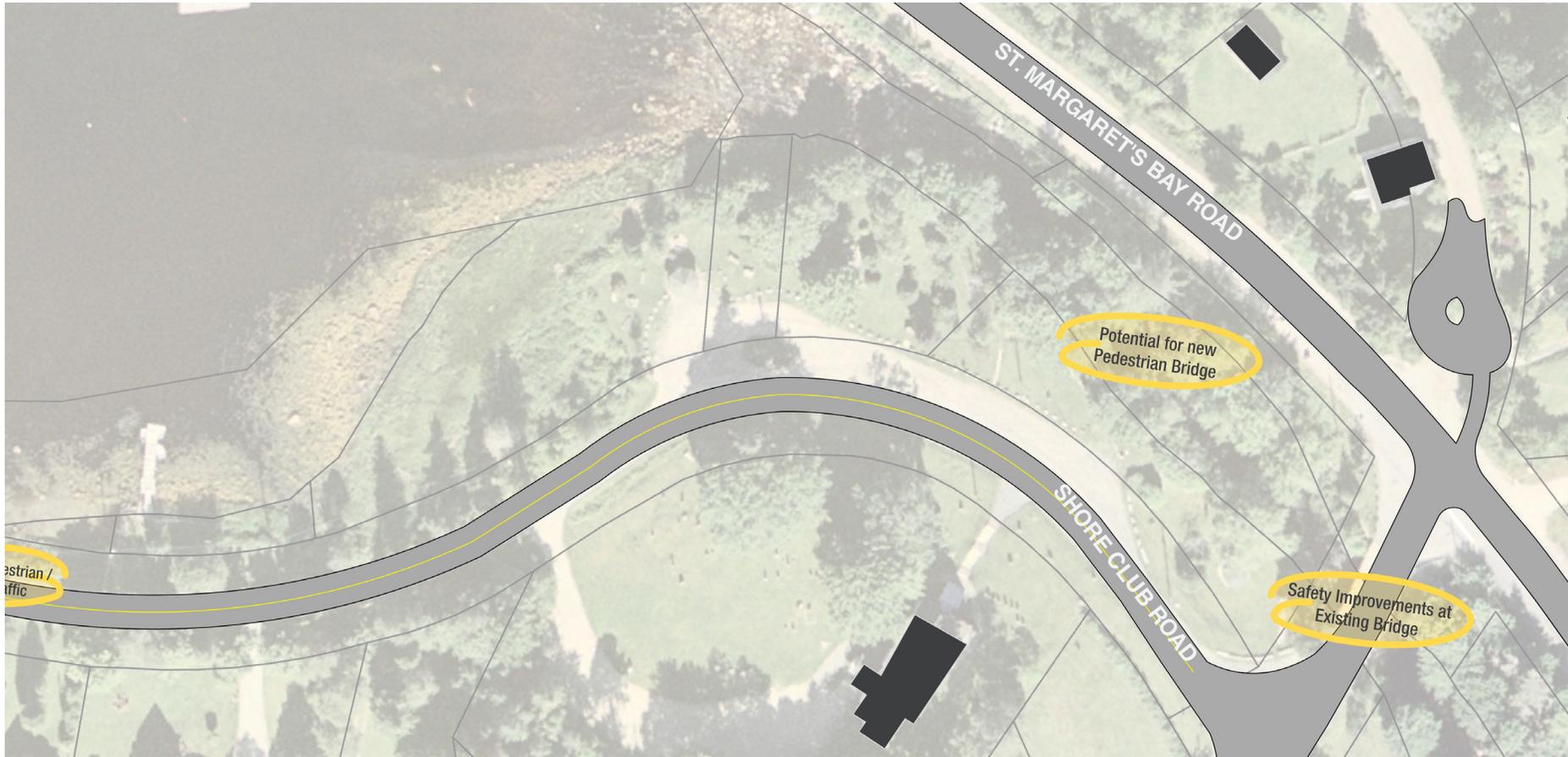
The proposed improvements for this section of St. Margaret's Bay Road include:

- + Realignment of the exit from Shankel Road, creating a 90-degree angle intersection with St. Margaret's Bay Road
- + Widening of St. Margaret's Bay Road to include the width of a third travel lane, which is used to create pedestrian safety islands and dedicated left-turn lanes to Shankel Road and the school
- + Articulation of crosswalk by safety island
- + Continuation of multi use path on northern side of road until school property, then connection to Rum Runners Trail to the west of the school
- + Removal of one redundant school driveway for better predictability of travel patterns



11. COVE CONNECTIONS + SIDEWALKS

11.1 SHORE CLUB ROAD (EASTERN PART)



EXISTING CONDITIONS

Shore Club Road is a vital connection along the south-eastern shore of Hubbard's Cove, as it is the only connector between St. Margaret's Bay Road and the amenity-rich area around the Shore Club.

It also attracts a lot of foot traffic due to its popularity and many nearby

attractions and accommodations. The mix of different transportation modes on this winding road with blind crests creates uncomfortable and dangerous traffic experiences for all road participants.

As previously mentioned in the 'Hubbard's Loop' section, the

access to Shore Club Road from St. Margaret's Bay Road also poses a challenge, as the narrow bridge over Hubbard's River leaves little room for safe pedestrian passage.

Bishop's Park is another integral feature of this area. The park affords stunning views of Hubbard's Cove

and is scenically situated between the Cove and the mouth of Hubbard's River. However, while the park does provide recreational value to its users, in its current state it does not support pedestrian passage through the site, especially for persons living with accessibility needs.



PROPOSED IMPROVEMENTS

The proposed improvements for this section of Shore Club Road include:

- + Following the goal to create pedestrian waterfront connectivity around the Cove, this concept introduces a southern sidewalk on St. Margaret's Road in addition to
 - + the multi-use path on the northern side. Extending from Yacht Club Road to Shankel Road, this southern sidewalk could connect to a dedicated pedestrian bridge
 - + The proposed pedestrian crossing over Hubbards River lines up with the entrance and
 - + the stairs of St. Luke's Church, thus creating a view corridor from St. Margaret's Bay Road.
- + An accessible pedestrian walkway meandering through the park, and connecting to a boardwalk, which will keep pedestrians in a safe distance from cars

11.2 SHORE CLUB ROAD (MIDDLE PART)



EXISTING CONDITIONS

Shore Club Road has a major advantage which sets it apart from other roads in Hubbards: long sections of its right of way are bordering directly on the water, with no private lands in between the road and the shoreline.

This circumstance can be taken advantage of: a boardwalk could

be installed on the seaward side of the road, beginning at Bishop's Park and ending just before the hill leading to the Tune Blue Inn.

Boardwalks should be generally seen through a critical lens, as sea level rise and increased storm events can cause damage. However, this

part of the shore is situated in a very sheltered corner of the Cove and has no direct exposure to the open waters of St. Margaret's Bay. Consequently, a thoughtful boardwalk design will have a good chance to last.

Aside from the recreational value of such an installation, the boardwalk

would also constitute a major safety improvement. With lanes width of approximately 3 metres and a lack of shoulders in many section of the road, this section of Shore Club Road is not suitable for pedestrian traffic in many locations in its current state.



PROPOSED IMPROVEMENTS

The proposed improvements for this section of Shore Club Road include:

- + A board walk with direct water access, which will add a recreational component to the walk
- + Separation of pedestrian and vehicular flows on the road
- + Landscaping, lookouts and seating opportunities integrated into the boardwalk design
- + Integrated launch docks for non-motorized water crafts (e.g. kayaks, canoes, stand-up paddleboards etc.)
- + Integration of boardwalk with sidewalks on both ends of the structure

11.3 SHORE CLUB ROAD (WESTERN PART)



EXISTING CONDITIONS

Following the section along the water, Shore Club Road winds up about 10 metres onto a gentle hill.

The right-of-way moves inland in this location, and private properties appear on the shoreline. Therefore, the boardwalk concept cannot be continued through this section.

The road is now flanked by businesses and residences on both sides of the road. This creates an increased rate of foot traffic.

Parking patterns add to the difficulty of navigating this area. Semi-formal parking lots belonging to businesses are arranged on intermittent

sections of shoulder on the road, while random parallel parking of vehicles is common. During larger concerts at the Shore Club, rows of parked cars back up to this area.

The blind crest combines a steep incline and a curve in the roadway, and is therefore one of

the most dangerous elements of this central section of the road.



PROPOSED IMPROVEMENTS

The proposed improvements for this section of Shore Club Road include:

- + Transition of the board walk into a regular sidewalk
- + Realignment of road in the vicinity of the blind crest,
- + including a separation of pedestrian and vehicular traffic
- + Crosswalk at the beginning of the Tuna Blue parking lot, where the sidewalk needs to switch sides
- + Continuation of sidewalk on southern side of the road to the intersection with Beach Road

11.4 SAILING CLUB AREA



EXISTING CONDITIONS

The Sailing Club is an anchor building for the community of Hubbards and is an activity hub for the most of the year. Cannery Beach is another popular destination just south of the Sailing Club.

Both sites cannot currently be reached by any active transportation routes.

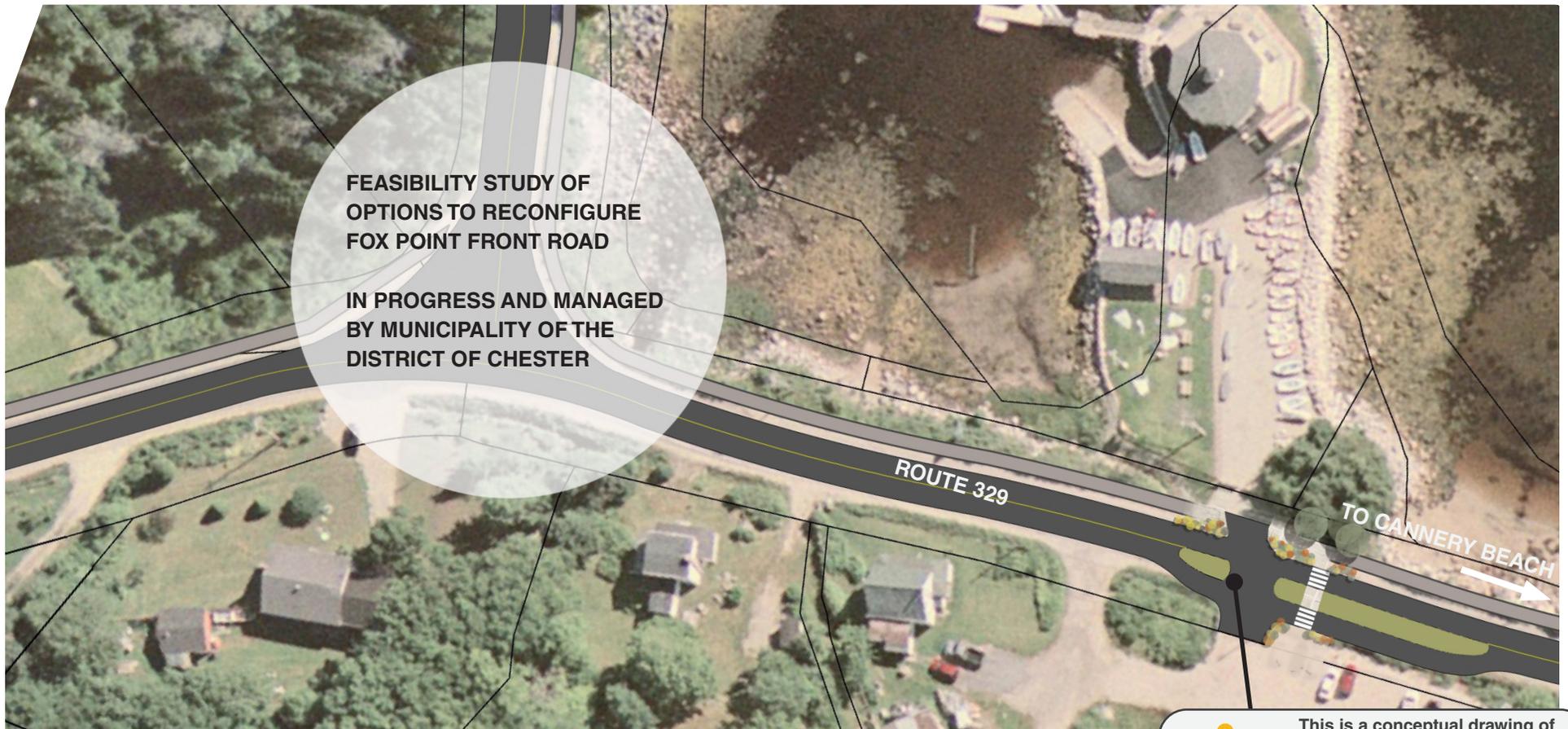
The sites either need to be reached with automobiles, or pedestrians need to navigate narrow shoulders of Route 320 or Fox Point Front Road.

Sailors arriving in the club with motorized vehicles need to park across the road. For many young sailors enrolled in various

programs, this road crossing can be particularly hazardous. This is therefore another site suitable for a gateway-style treatment.

The Municipality of the District of Chester is currently running a feasibility study on several options to introduce more active

transportation modes on Fox Point Front Road. The results of the study are not available yet, but will form an essential consideration for any intersection realignments in this area.



PROPOSED IMPROVEMENTS

The proposed improvements for this section of Route 329 include:

- + Pedestrian safety island improving crossings and marking the gateway in the Hubbards area for drivers coming from the Aspotogan Peninsula
- + New sidewalks from the Hubbards Shopping Centre (on Route 329) and potentially on Fox Point Front Road (depending on results of feasibility study)
- + Newly realigned intersection of Fox Point Front Road and
- Route 329 (depending on results of feasibility study)
- + New sidewalk from the corner of Route 329 / Fox Point Front Road to Cannery Beach



This is a conceptual drawing of how pedestrian safety principles could be implemented. The entire intersection will need detailed design and is subject to change.



12. INTERIM PROJECTS

INTERIM PROJECT NO.

1

ADD TEMPORARY BUMP-OUTS TO CROSSWALKS AND INTERSECTIONS

IDEA:

The Halifax Regional Municipality has accumulated expertise and experience in the creation of temporary pedestrian improvements over the last few years. Many sites around Halifax have received added pedestrian safety zones where road surfaces of street intersections are either underutilized or unused. This practice could be extended to Hubbards, in order to create immediate safety benefits and to observe the function of changed intersections layouts, before creating permanent fixtures.

POTENTIAL LOCATIONS:

- + In front of sailing club
- + At crosswalks by The Barn and J.D. Shatford Elementary School

BENEFITS:

- + Items such as bollards, barriers or planters are low-cost, and can be easily removed
- + Interim measures will give the community some experience with redesigns prior to construction of permanent measures, which will help to create better long-term solutions



Design of solutions acceptable to the Province and road owner to be verified.



Figure 66: Examples of temporary bump-outs in Halifax

INTERIM PROJECT NO.

2

PAINT SOME ROAD MURALS

IDEA:

Sometimes all it takes to completely change the function of a street or intersection is some paint. This may not be conceivable for St. Margaret's Bay Road or Route 329, but on minor roads in the community it may be possible. These types of 'road murals' are certain to catch drivers eyes and increase their level of attention driving. The good thing about such paintings is that nothing about the road configuration needs to change, especially since minor intersections in Hubbards are often completely unmarked.

POTENTIAL LOCATIONS:

- + Intersection of Shore Club and Beach Roads
- + Yacht Club Road

BENEFITS:

- + Very low cost for implementation
- + Possibilities to delineate suggested pedestrian realms
- + Elements of placemaking and community beautification



Figure 67: Neighbourhood Intersection Art Project (Image Credit: Halifax Regional Municipality)

INTERIM PROJECT NO.

3

CREATE SMALL OPPORTUNITIES FOR INTERACTION

IDEA:

The community of Hubbards should try concepts of temporary placemaking that increases social interaction. Sometimes just a few chairs and planters can create a cozy corner that invites people to sit down and talk to each other. It would be great if street elements could be added, that encourage forms of interaction that go beyond talking. Chess boards, street libraries for book exchange or music instruments can all play a role in activating temporary plazas.

POTENTIAL LOCATIONS:

- + In front of Aspotogan Heritage Trust
- + Corner of St. Margaret's Bay Road with Fox Point Front Road
- + Lawn on hilltop of community waterfront property

BENEFITS:

- + This activity reclaims public places, encourages more persons to stop and join
- + Heartwarming details make walking more interesting
- + Free amenities in public spaces help with equity in a community



Figure 68: Street libraries can add a little touch to a plaza;
Image credit: John (CC-BY SA 2.0)

INTERIM PROJECT NO.

4

ADVANCE STRATEGIC LAND ACQUISITIONS

IDEA:

Some popular destinations are too far away from the community centre to justify the expense of a new trail construction. Queensland Beach is an example of such a site. While it is frequently visited by residents of Hubbards, the 2 km of distance from the elementary school make investments into a dedicated multi use path not viable. Instead, the Halifax Regional Municipality could wait for opportunities when connective lands between the trail and the beach go on sale or tax sale.

Such lands could either remain with the Municipality or be re-offered to the real estate market after a public easement was registered. Where no trail design is available for such connectors, an informal path could be cleared on such lands to allow for access in the interim.

POTENTIAL LOCATIONS:

- + Between Queensland Beach and Rum Runners Trail
- + Between Hubbards Beach and Schwartz Road
- + Between Schwartz Road and Conrads Road
- + Between the Wight Nature Preserve and the Recreation Centre

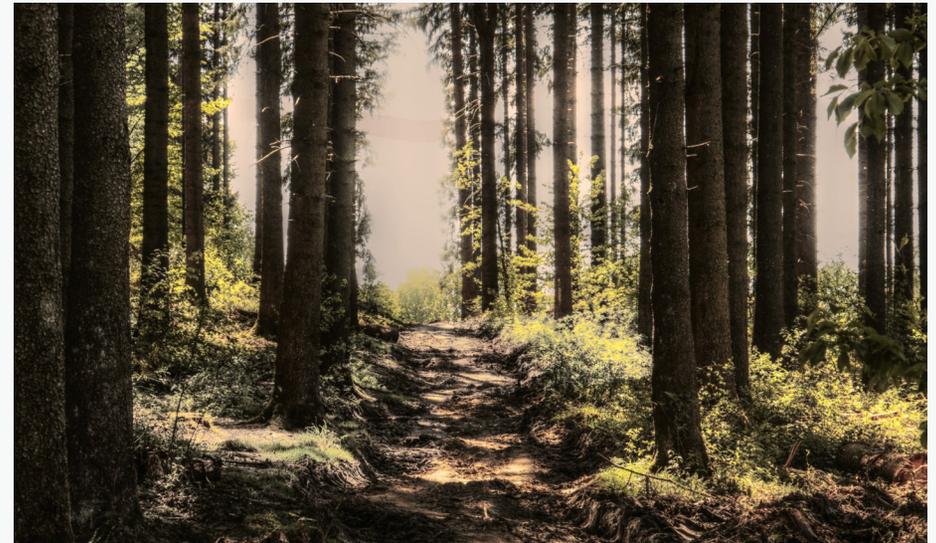


Figure 69: Informal trails over newly secured land can provide interim solutions before lasting active transportation infrastructure is designed.

INTERIM PROJECT NO.

5

BUILD INTERIM CROSSWALKS

IDEA:

While the full implementation of this plan will likely take several years, pedestrian safety in key locations of Hubbards could immediately be improved through the installation of a few temporary crosswalks. In the summer of 2022, such a crosswalk including signal beacons was installed in front of the JD Shatford Memorial Library. Even though this installation has been placed in the context of the 49er World Sailing Championship, the lack of pedestrian safety in Hubbards could warrant a replication of this approach until final solutions are designed.

POTENTIAL LOCATIONS:

- + In front of JD Shatford Memorial Library (long-term)
- + In front of Sailing Club
- + Between Hooked Rug Museum and Recreation Centre (thus connecting to Rum Runners Trail through newly created Vinegar Lake Road right-of-way)
- + At Queensland Beach, in connection with a connector trail over Crown property PID# 40236010 to the Rum Runners Trail



Figure 70: Temporary crosswalk in front of JD Shatford Memorial Library in the summer of 2022.

BENEFITS:

- + Low budget improvement with high impact
- + Possibility to track usage and refine locations before final design



13. DEVELOPMENT POLICIES

13.1 PURPOSE OF DEVELOPMENT POLICY PROPOSALS

The final element of the 'Six Big Moves' consists of proposals for updated development policies in the Hubbards project area. Municipal land use planning is primarily within the authority of the Municipality of the District of Chester and Halifax Regional Municipality, respectively. Consequently, these development policy proposals are suggestions to be considered by municipal Councils and planning staff during the next expedient occasion to amend planning rules for the area. Ideally, this should occur around the same time in both municipalities, in order to prevent widely differing rules on both sides of the municipal boundary.

The analysis of current land use rules applicable to the project area (Sections 5.11 and 5.12) revealed two problematic aspects of the current plans. First, although Hubbards is a distinct community which functions as a whole across municipal boundaries, the planning rules change significantly at the municipal boundary line. Secondly, planning rules of the Hubbards Community Plan project area falling under the authority of the Halifax Regional Municipality area very 'broad-brush' and lack necessary detail to guide development in a community as vibrant and intricate as Hubbards.

Based on these insights, the following sections will outline development rules which could be adopted in both municipalities to facilitate a more cohesive form of community development with more attention to details.

The recommendations for these planning rule adjustments will be categorized by the community vision designations earlier established in section 6.1, which cluster Hubbards into eight areas of distinct character.

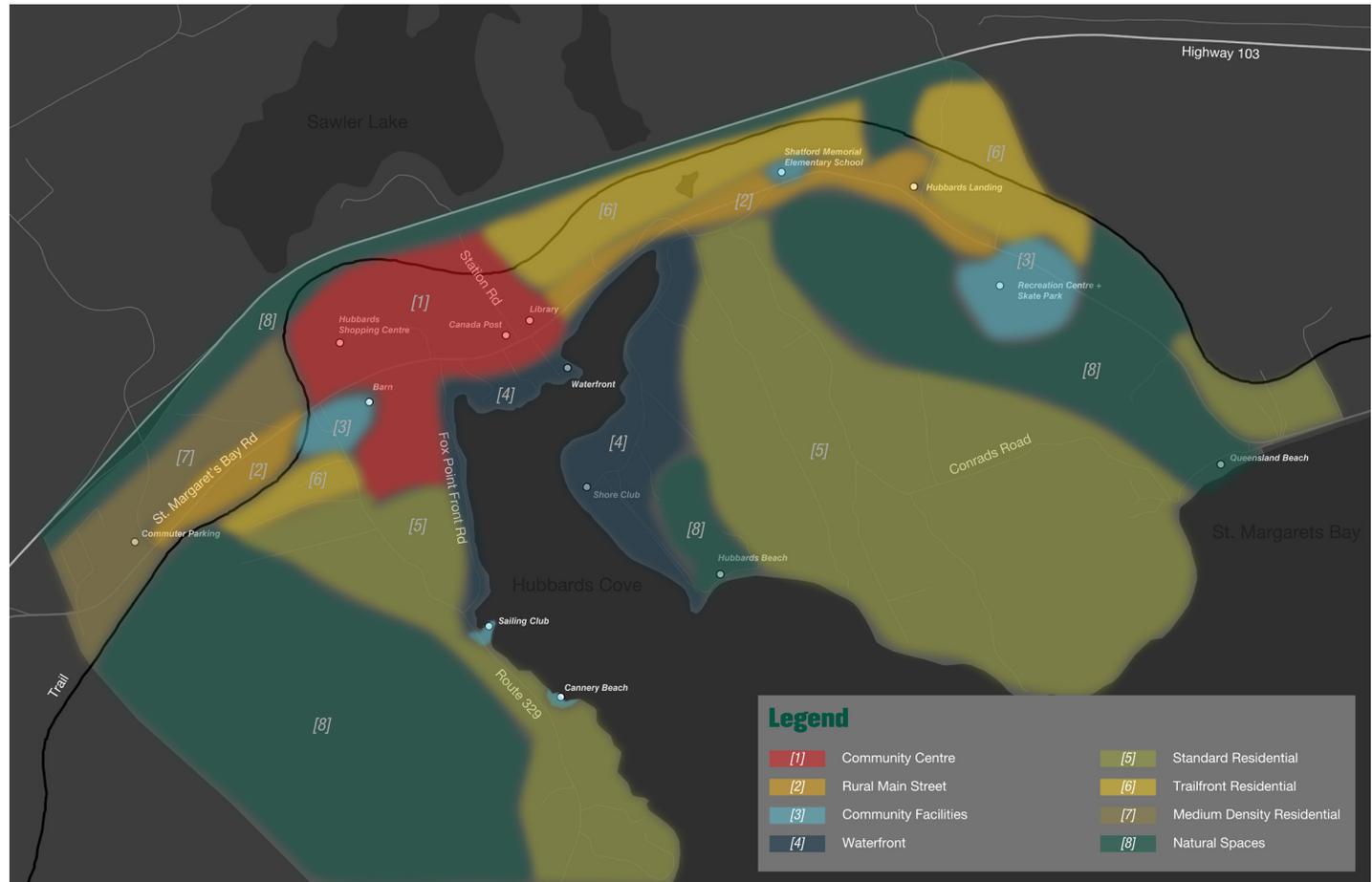


Figure 71: Community Vision Map from Part II of the Community Plan

13.2 COMMUNITY CENTRE DEVELOPMENT POLICIES

The Community Centre designation is a prime area to allow for growth of a traditional main street. Land use planning for this part of the community therefore needs to incorporate rules with a strong design focus, accentuating the character of current buildings and ensuring that future development is compatible and complementary to the current streetscape.

Land uses in this area should prioritize residential properties and commercial uses which are smaller in scale and targeting pedestrians. Car-centred land uses such as gas-stations, garages and drive-throughs or other forms of buildings that do not facilitate human interaction (e.g. self-storage) should be prohibited.

Proposed Rules and Policies

BUILDING SETBACKS

- + 50% of a building's front facade should be parallel to the street and within 2 and 5 m of the front lot line to allow for a continuous 'street wall'.
- + Larger front setbacks may be permitted for buildings with outdoor public spaces, decks, patios and verandas.
- + Single family dwellings should be built within 10 m of the front lot line.
- + Street corners need to be defined by buildings and cannot stay vacant unless professionally landscaped.
- + Where multiple buildings on a lot are allowed and planned, a building can be exempt from the street wall requirement if it is set back by at least 25 m from the lot frontage, and if the property's site plan leaves room for the development of a future building closer to the street.

SITE PLANNING

- + Large areas of uninterrupted parking should be avoided. The parking lots should not have more than 20 stalls in any

direction without an interruption by landscaping of at least 2 m width.

- + Landscaped buffers of at least 1 m shall separate parking lots and driveways from any lot line.
- + Commercial lot area which is not dedicated to buildings, parking, walkways or human activity should include one tree per 50 m².

BUILDING DESIGN

- + Windows on all facades should vertically and horizontally align with each other, and other features of the building
- + At least 60-75% of the building frontage on the ground floor should be glazed, allowing views of the building use and creating visual interest for pedestrians. Second floor commercial window coverage should be minimum 40-50%.
- + All new developments should have their main commercial entrance fronting the street. Residential or additional entrances may be located at the side or rear of the building.

+ All commercial entrances should be ornamented by one of the following:

- Awnings
- Emphasized door lintels
- Cantilevered roof
- Pilasters on the sides of the entrance
- Or none of the above if the entire ground floor is visually separated from upper floors by projecting string courses or cladding.

+ Buildings shall have a minimum streetwall height of 6 m.

+ After the maximum streetwall height of 8 m, a setback of 3 m is required or the next

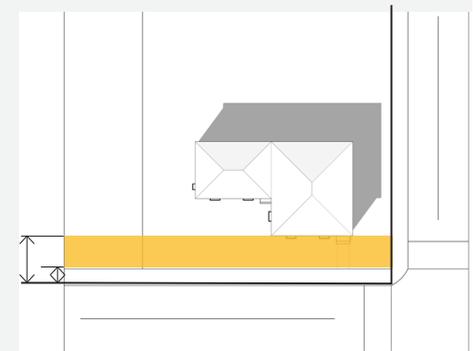


Figure 72: Street wall setbacks

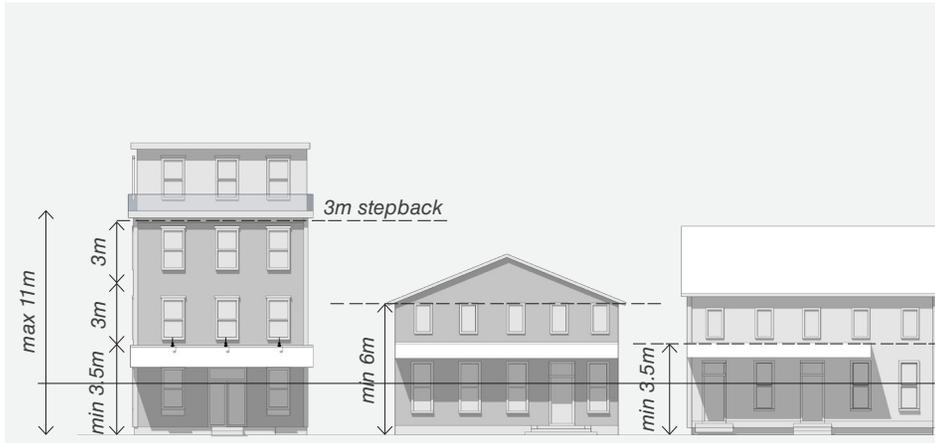


Figure 73: Streetwall and stepback



Figure 74: Facade articulation

storey must be developed within the attic of a roof.

- + A minimum floor-to-floor height of 3.5 m (or equivalent to the adjacent property) must be maintained at street level to provide a strong street presence and pedestrian interest.
- + Stories above the ground floor should have a floor-to-ceiling height of minimum 3 m.
- + Buildings should typically be divided vertically and/or horizontally into symmetrical façade units called bays. This can be done through recession or projection of walls, and symmetrical spacing of pilasters and columns.
- + Facades should be broken into sections of up to 12 m (if they exceed that width), using architectural elements such as projections, recesses, awnings, color, texture, and/or landscaping to reduce the continuous massing of the front elevation.
- + In the first storey, blank walls without any articulation or windows may not exceed a horizontal length of 2-5 m.
- + New mixed-use buildings should have identifiable vertical

articulation that distinguishes the ground floor from the upper floors. For example: projections, change in materials, window treatments, string courses after first floor, signage bands etc.

ROOF SHAPES

- + Roof pitches should be between 4:12 and 9:12. Roofs that are incompatible with the building styles and visual continuity of the streetscape should be avoided.
- + Roof pitches below 4:12 may be still permitted if they contain green roofs, and/or landscaping designed by a landscape architect.
- + Roof pitches above 9:12 may be permitted in case of gambrel roofs which are open on two sides of the buildings.

SIGNAGE

- + Well-designed signs on building facades (whether as fascia signs or projecting signs) should be promoted while ground signs should be limited. Mobile signs with neon-coloured lettering can be particularly disruptive to the visual experience.

13.3 RURAL MAIN STREET POLICIES

Development policies for the Rural Main Street designation should have similarities to the Community Centre designation. However, with larger lots and rather loosely defined building patterns along the road, design requirements can be less stringent in this designation to allow for more flexibility in the placement of buildings and arrangement of properties (i.e. site planning). For example, some parking space in the front yard of buildings is better suited for this type of development.

With regard to land uses, the rural main street designation can also accommodate a wider range of businesses. Commercial developments targeting motorists can fit into this environment, as long as the building design reflects the rural feel of the area. The nearby Town of Mahone Bay features some good examples of larger chain stores or restaurants that adapted building styles or signage to the historic downtown aesthetic.

Proposed Rules and Policies

SITE PLANNING

- + Parking areas in front of buildings should be sized in proportion to the building. The depth of parking areas in the front yard should not exceed the width of the building by a factor of 2:1.
 - + Large areas of uninterrupted parking should be avoided. The parking lots shall not have more than 20 stalls in any direction without an interruption by landscaping of at least 2 m width.
 - + Landscaped buffers of at least 1 m shall separate parking lots and driveways from any lot line.
 - + Unpaved parking areas are preferred to maintain the rural look. Demarcation of individual parking stalls is encouraged through usage of natural landscaping materials such as wood and rock.
 - + Pedestrian priority areas should be defined through landscaping elements and be used to define walkways and sidewalks which can be clearly differentiated from areas where pedestrians may encounter vehicular movement.
- + The main entrances should have direct access from the closest sidewalk (no more than 125% of the shortest distance between entrance and sidewalk).

BUILDING DESIGN

- + All new developments should provide a clearly defined pedestrian entrance. The main entrance must be visually distinct from other openings in the street wall.
- + All new developments should have their main commercial entrance fronting the street. Additional entrances may be located at the side or rear of the building.
- + Where garages are integrated into the main building on a lot, garage doors facing the lot frontage (i.e. in the front elevation of a building) may not exceed 15% of the building's front elevation, unless they are set back by at least 10 m from the building wall closest to the lot frontage.
- + Roof pitches should be between 4:12 and 9:12. Steeper roof pitches or oversized roofs that are incompatible with the building

styles and visual continuity of the streetscape should be avoided.

- + Roof pitches above 9:12 may be permitted in case of gambrel roofs which are open on two sides of the buildings.
- + At least 60-75% of the building frontage on the ground floor should be glazed, allowing views of the building use and creating visual interest for pedestrians. Second floor commercial window coverage should be minimum 40-50%.

SIGNAGE + LIGHTING

- + Natural signage materials (e.g. wood or stone) should be preferred over artificial materials.
- + Lighting should be incorporated in the signage and wayfinding elements wherever appropriate.
- + Spacing of wall-mounted lighting fixtures must be at regular intervals.
- + A single row of lighting (both wall-mount and free standing) must not include more than one style of lighting fixture.

13.4 COMMUNITY FACILITIES

Developments within the Community Facilities designation are generally owned by the Province, one of the municipalities or a non-profit organization. Since the goal of the operation of these properties is to serve the public, and because development decisions on such lands are generally made through government or civil society bodies, this designation does not require a great amount of prescriptive rules to attain the desired effects for the community.

However, some guidelines for good public spaces are worth keeping in mind when new facilities are developed or when existing facilities are altered. The following policies are based on the public feedback received as part of this plan and can help with new developments in this category.

Proposed Rules and Policies

- + When public facilities are developed or updated, they should consider the public feedback from the Hubbards Community Plan by
 - aiming to use every development as an opportunity to increase the community's public space assets through innovative placemaking design that beautifies Hubbards and encourages interaction between residents.
 - promoting active transportation for all ages and abilities through design elements such as seating opportunities, night-time lighting, drinking fountains, shading canopies and other features increasing the accessibility of spaces.
- + Public facilities need to be accessible by active transportation and make contributions to connectivity throughout the community by:
 - utilizing publicly owned spaces for trail connections and ease of movement throughout the community (good example of this practice is the connector trail across the elementary school property).
 - contributing to active transportation connections shown on the AT needs map in Section 6.1, and also considering the main proposed AT routes when selecting locations for new community facilities.



13.5 WATERFRONT DEVELOPMENT POLICIES

Developments along the waterfront are an important part of the overall visual image of Hubbards Cove, and therefore contribute significantly to the overall character of the community. Most of the Cove's shoreline is plainly visible from the community waterfront and other key locations in Hubbards.

The types of buildings that create a picturesque backdrop to the cove waters are more challenging to define than in the case of main street buildings. However, buildings in this area do generally need to be designed to high standards.

Waterfront access is another important planning consideration for the Waterfront Designation, which was highlighted by many participants of the public engagement within this project.

On regular residential properties, there are few options to facilitate public waterfront access. However, where larger developments trigger planning application procedures, municipalities are in the position to negotiate public access easements as parts of development proposals.

The wide stretch of waterfront properties in Hubbards also necessitates policies that take climate change and sea level rise into account.

Proposed Rules and Policies

PLANNING APPLICATIONS

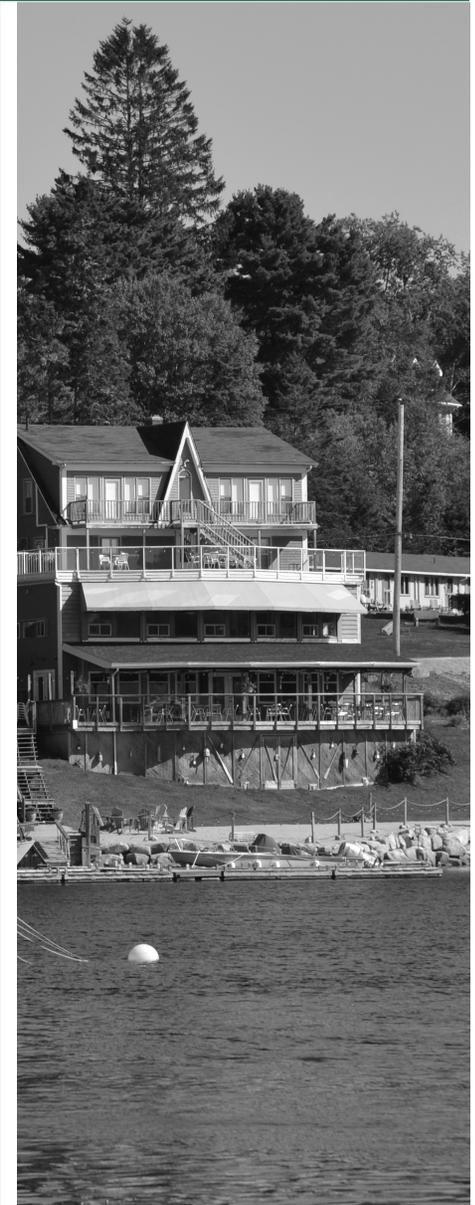
- + Commercial developments up to 7,500 square feet on the side of the Halifax Regional Municipality, and up to 20,000 square feet on the side of the Municipality of the District of Chester, which are currently permitted without special regulations, should require a site plan approval procedure within the waterfront development designation.
- + Commercial developments including tourist housing as well as multi-unit residential developments on waterfront lots should have development agreement or site plan approval policies which facilitate negotiations about public waterfront access as a form of public benefit.
- + Policies should seek heightened building design requirements for developments under planning applications which face the waterfront of Hubbards Cove.
- + Sight lines from key places in the community need to be protected by view corridors, which are kept free from development and allow for water views.

BUILDING DESIGN

- + The buildings must have the same color scheme in all the facades.
- + Front and side elevations should have a consistent type and quality of materials and windows. Exterior cladding materials of new developments should be compatible with the architectural style of the building and adjacent buildings.
- + All materials must be high quality, durable and appropriate for the coastal climate.
- + Green roofs and terraces with plantings are encouraged.

CLIMATE CHANGE

- + More detailed requirements related to climate change need to be considered, e.g. horizontal and vertical setback requirements from the water.



13.6 RESIDENTIAL POLICIES

Hubbards features several residential areas which have grown over long periods of time and display established patterns of land use. Other areas are still in transition or vacant, but can be expected to absorb growth after the completion of the Highway 103 twinning.

The Community Vision Map distinguishes between three types of residential areas that deserve separate attention. Standard residential areas are similar to suburban development and unlikely to transform in the near future. However, they should be generally open to some forms of gentle infill development such as cluster housing, which is currently not a permitted use in the R2 Zone of the Halifax Regional Municipality.

As in the case of previously mentioned designations, planning policies for larger developments should be applied to make best use of existing active transportation infrastructure and complement it with connector trails wherever possible.

The trailfront residential designation plays a special role in this context, as further detailed in the analysis of Section 6.7. By creating residential density around the trail, this type of development could increase trail safety and usage.

Proposed Rules and Policies

STANDARD RESIDENTIAL

- + In HRM: add document provisions to allow for gentle density increases such as:
 - triplexes and fourplexes subject to special building design requirements.
 - introduce enabling policies or as-of-right procedures (e.g. site plan approval) for clustered housing implemented for up to 10 small dwellings resembling backyard suites (as bare land condominiums with common areas and shared parking).
- + In the District of Chester: add document policies (for site plan approvals, development agreements) that require integration of developments into active transportation infrastructure wherever possible, based on the plans of this document.

TRAILFRONT RESIDENTIAL

- + Introduce new a Trailfront Residential Zone, which features a maximum 'front yard' setback from the trail, a requirement to have main entrances on the trail and service entrances on backyard alleys, as well as building design requirements which support an 'eyes on the trail approach'.
- + Landscaping requirements for yards fronting the trail should promote natural appearance, tree retention within 5m of the trail and prevent an overly developed or 'urban' look of properties.
- + On the side of the Halifax Regional Municipality, amend the Conservation Subdivision Design Policies of the Regional Plan to require an intergration of the Rum Runners Trail in designs of subdivision proposals in these areas.
- + Amend policies for site plan approval procedures and planning applications to require permeability between the trail and other community destinations for most types of developments.

MEDIUM DENSITY RESIDENTIAL

- + Create zoning focusing on affordable housing forms and prohibiting low-density developments on large lots.
- + Ensure ease of access to Rum Runners Trail and Hubbards Shopping Centre through various planning tools available through the Municipal Government Act.
- + Adopt design guidelines that maintain a rural appearance of buildings for multi-unit dwellings, seniors housing or licensed residential care facilities.

13.7 NATURAL SPACE POLICIES

The project area of the Hubbards Community Plan contains a number of ecosystems with important ecological and functional roles. In some cases, both roles are combined in the same natural asset.

For example, the strip of woods separating the community of Hubbards and Highway 103 is a continuous natural habitat, which at the same time acts as a noise buffer from the highway.

The Hubbards River and Shankels Pond are two significant natural features in the heart of the community. Further areas of elevated ecological value within the project area include beaches, barachois ponds and wetlands. The latter are present at Miller's Marsh, west of the 'Hubbards Landing' commercial development and within the Wight Nature Preserve.

While some of the natural habitats are within public ownership and therefore enjoy a high degree of protection, others are situated on private lands and may need special attention on zoning maps.

Proposed Rules and Policies

- + Zoning maps within the Halifax Regional Municipality and the Municipality of the District of Chester should be reviewed to reflect a finer level of protection for ecologically valuable areas in Hubbards.
- + A plan review should determine whether current setback requirements from waterways and wetlands are sufficient for the needs of a rural centre like Hubbards.
- + Within the buffer of woods between Highway 103 and the community of Hubbards, applications for any form of development should include a site plan showing how the tree cover of this area will be maintained to be maximum extent possible.





14. IMPLEMENTATION

14.1 PHASING OF THE INFRASTRUCTURE COMPONENTS

In order to help turning this plan into reality, it is very important to assign approximate cost estimates to the suggested design solutions and to assign priorities to various elements of this plan.

This plan introduced many concepts and ideas for Hubbards which are not feasible to be implemented all at the same time. However, the order of improvements in Hubbards matters, as some of the proposed projects are foundational for other steps of the plan.

The infrastructure proposals have therefore been staged into a total of five phases. The first phase is the most essential component of the project, as it introduces basic connectivity between the east and west of Hubbards. Phases 2 and 3 also implement integral parts of the 'Big Moves' proposed in this document, although the sequence between these two phases is likely interchangeable.

Phase 4 would implement further important aspects of active transportation in Hubbards. Finally, Phase 5 of the project focuses on active transportation improvements of an optional character, which would be desirable from a recreational point of view and significantly contribute to the community's beautification.

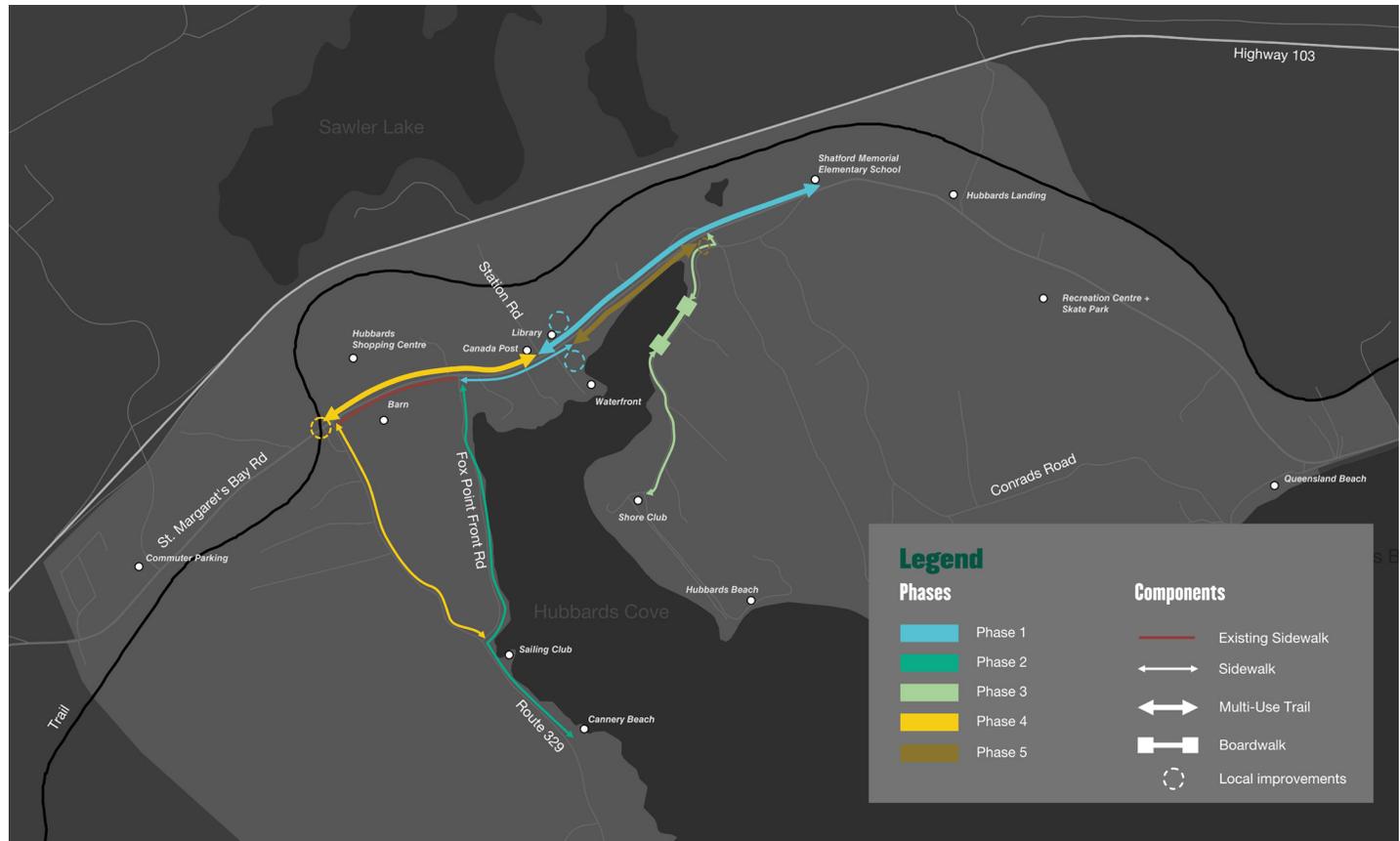


Figure 75: Overview map with all implementation phases

It should be noted that all cost indications on the following pages represent preliminary opinions of probable costs which are based on the current high-level, conceptual design. They are intended for initial budgetary discussions only.

A contingency is included for each estimate to allow for unexpected issues that may arise in construction or future design phases. This opinion of probable costs is presented based on experience, qualifications, and best judgment. Market trend changes, non-competitive bidding situations, unforeseen labour and material

adjustments, availability and the like are beyond control and are not warranted or guaranteed. Actual costs may vary from the opinion provided.

All values are provided in 2022 Canadian dollars and allowances should be made for inflation.

14.2 PHASE 1

Creating an active transportation spine on St. Margaret's Bay Road is the first logical step to turn Hubbards into a walkable community. All other pedestrian and biking connections will grow from this main artery and connect to it.

The end of the existing sidewalk on the street corner with Fox Point Front Road is recommended as the starting point of improvements. From here, the sidewalk would be extended along the southern side of St. Margaret's Bay Road to the Waterfront Park.

On the northern side of the road, the proposed multi-use pathway could be constructed between Station Road and the Elementary School during this phase. This newly constructed path would ensure continuous connectivity between the school and the shopping centre. At the same time, it would also create the first section of the 'Hubbards Loop', with two connecting points to the Rum Runners trail at the school and at Station Road.

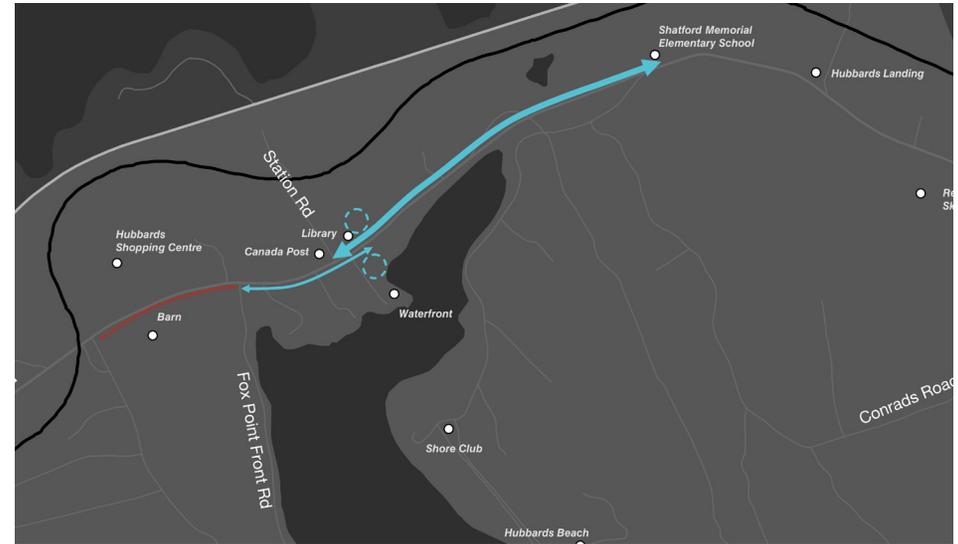


Figure 76: Map of Phase 1

Transportation Improvements	Unit	Unit Cost	Quantity	Price
3.5m Wide Paved Multi Purpose Trail (Asphalt, Gravels, Traffic Control, etc.)	m	\$500	1150	\$575,000
2 - 0.6m Wide Concrete Curb and Gutter (Concrete Curb, Gravels, Traffic Control, etc.)	m	\$280	1450	\$406,000
2 - 2m Wide Grassed Boulevards	m	\$375	1450	\$543,750
2 - 3.5m Wide Paved Travel Lanes (Asphalt, Gravels, Traffic Control, etc.)	m	\$800	1450	\$1,160,000
1 - 3.5m Wide Median / Turning Lane	m	\$450	300	\$135,000
2.0m Wide Concrete Sidewalk	m	\$280	440	\$123,200
Stormwater main	m	\$425	1450	\$616,250
Stormwater catchbasins	qty	\$15,000	36	\$540,000
General Contract Requirements (Mob, Demob, Environmental Controls, Permitting, Coordination)	sum	\$30,000	1	\$30,000
SUBTOTAL				\$4,129,200
Design + Contingencies (30%)				\$1,238,760
TOTAL				\$5,367,960

Apart from the improvements to active transportation, Phase 1 also encompasses two placemaking proposals which will increase the vibrancy of the central section of St. Margaret's Bay Road in Hubbards.

The creation of two public spaces are integral to the streetscape concept and have been situated within this plan on the upper end of the Waterfront Park property and next to the JD Shatford Memorial Library.

It is very important to implement

these placemaking proposals at the same time as the transportation improvements, since residents in Hubbards will need attractive destinations that will encourage pedestrian trips.

Lively space along the edge of St. Margaret's Bay Road will also help vehicle drivers to recognize this area as the centre of the community, thus making a contribution to reduce travel speeds in this central location of Hubbards.



Figure 77: Detail of placemaking locations

Placemaking Improvements	Unit	Unit Cost	Quantity	Price
Shape Subgrade	sum	\$50,000	1	\$50,000
Plaza construction	m2	\$2,000	600	\$1,200,000
Electrical services to vending site in waterfront park	m2	\$400	50	\$20,000
Sodding allowance	m2	\$18	100	\$1,800
Trees, shrubs, plantings allowance	sum	\$25,000	1	\$25,000
Site furniture - seating, tables, swing hammocks allowance	sum	\$100,000	1	\$100,000
General Contract Requirements (Mob, Demob, Environmental Controls, Permitting, Coordination)	sum	\$30,000	1	\$30,000
SUBTOTAL				\$1,426,800
Design + Contingencies (30%)				\$428,040
TOTAL				\$1,854,840

14.3 PHASE 2

Following the creation of basic active transportation infrastructure on St. Margaret's Bay Road, the second recommended step is to implement the 'Cove Connections' concept introduced in this document.

Both Phases 2 + 3 would create walking infrastructure and connectivity around Hubbards Cove. Each of these two phases represents one shore of Hubbards Cove.

Between the two sides of Hubbards Cove, the western shore appears to require a slightly higher priority due

to the presence of the Sailing Club, Cannery Beach and frequent trips of youth and children to these two destinations. The western shore is therefore addressed through Phase 2.

While the municipal feasibility study for Fox Point Front Road is still underway and a conceptual design solution is still to be found for active transportation on that road, it is assumed for the purposes of this cost estimate that a regular sidewalk can be accommodated within the road's right-of-way.

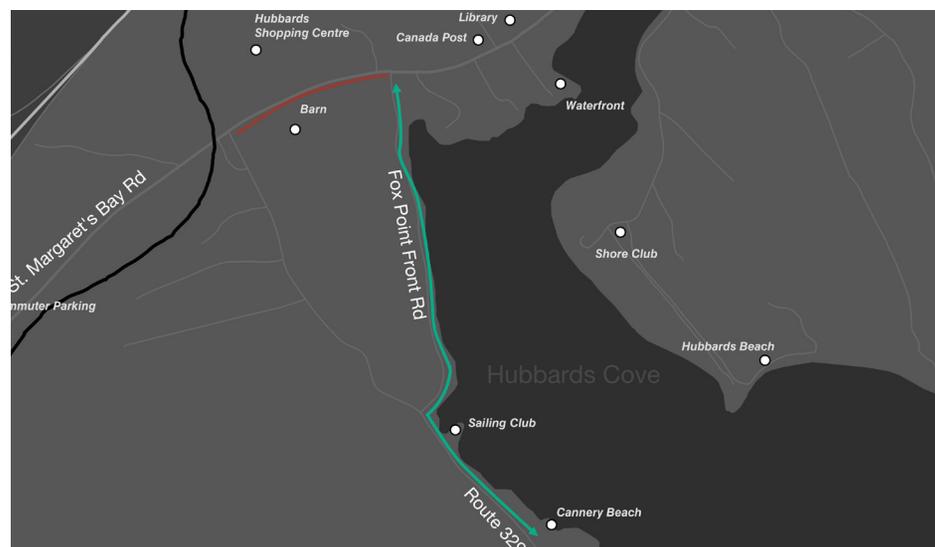


Figure 78: Map of Phase 2

Transportation Improvements	Unit	Unit Cost	Quantity	Price
2 - 0.6m Wide Concrete Curb and Gutter (Concrete Curb, Gravels, Traffic Control, etc.)	m	\$280	1400	\$392,000
1 - 2m Wide Grassed Boulevards	m	\$200	1400	\$280,000
2 - 3.5m Wide Paved Travel Lanes (Asphalt, Gravels, Traffic Control, etc.)	m	\$800	1400	\$1,120,000
2.0m Wide Concrete Sidewalk	m	\$280	1400	\$392,000
Stormwater main	m	\$425	750	\$318,750
Stormwater catchbasins	qty	\$15,000	34	\$510,000
General Contract Requirements (Mob, Demob, Environmental Controls, Permitting, Coordination)	sum	\$30,000	1	\$30,000
SUBTOTAL				\$3,042,750
Design + Contingencies (30%)				\$912,825
TOTAL				\$3,955,575

14.4 PHASE 3

The recommended third phase to implement the Hubbards Community Plan would consist of creating pedestrian infrastructure on the eastern side of Hubbards Cove, mostly along Shore Club Road.

This would complete the horseshoe-shaped pedestrian infrastructure around Hubbards Cove, and enable walking access to most recreational and tourist facilities along the waterfront.

The creation of a boardwalk along parts of Shore Club Road which front on the water will also enhance the scenic value of the community.



Figure 79: Map of Phase 3

Transportation Improvements	Unit	Unit Cost	Quantity	Price
2 - 0.6m Wide Concrete Curb and Gutter (Concrete Curb, Gravels, Traffic Control, etc.)	m	\$280	765	\$214,200
1 - 2m Wide Grassed Boulevards	m	\$200	765	\$153,000
2 - 3.5m Wide Paved Travel Lanes (Asphalt, Gravels, Traffic Control, etc.)	m	\$800	765	\$612,000
4.0m Wide Boardwalk	m	\$320	385	\$123,200
2.0m Wide Concrete Sidewalk	m	\$280	765	\$214,200
Cantilevered, widened shoulder for pedestrians over Shankel Rd bridge	m ²	\$3,500	36	\$126,000
Stormwater main	m	\$425	765	\$325,125
Stormwater catchbasins	qty	\$15,000	19	\$285,000
General Contract Requirements (Mob, Demob, Environmental Controls, Permitting, Coordination)	sum	\$30,000	1	\$30,000
SUBTOTAL				\$2,082,725
Design + Contingencies (30%)				\$624,818
TOTAL				\$2,707,543

14.5 PHASE 4

This recommended fourth phase closes the 'Hubbards Loop' and adds an important connection from the 'additional links' category of the 'Active Transportation Needs' map. It also completes the goals of the 'Sidewalk Extensions' theme of this plan's 'Six Big Moves'.

The main component of this phase is the completion of the multi-use path on the northern side of St. Margaret's Bay Road. After the first part of that path is completed in Phase 1, this phase would entail finishing of the remainder section from Station

Road to the Rum Runners Trail crossing of St. Margaret's Bay Road. Placemaking improvements in that area and safety improvements of the trail crossing are also included.

Finally, a creation of a sidewalk on Route 329 between St. Margaret's Bay Road and Fox Point Front Road would complete this phase.

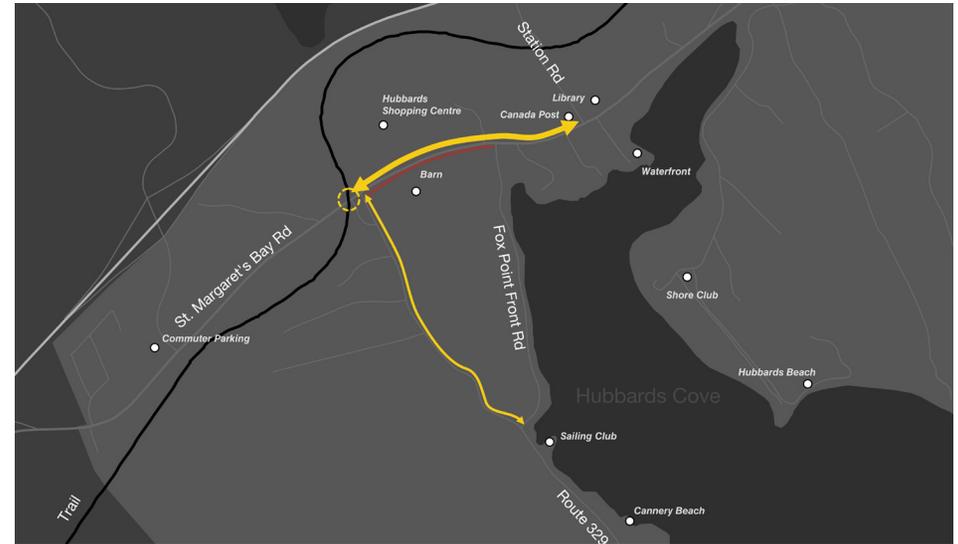


Figure 80: Map of Phase 4

St. Margaret's Bay Improvements	Unit	Unit Cost	Quantity	Price
3.5m Wide Paved Multi Purpose Trail (Asphalt, Gravels, Traffic Control, etc.)	m	\$500	600	\$300,000
2 - 0.6m Wide Concrete Curb and Gutter (Concrete Curb, Gravels, Traffic Control, etc.)	m	\$280	650	\$182,000
2 - 2m Wide Grassed Boulevards	m	\$375	650	\$243,750
2 - 3.5m Wide Paved Travel Lanes (Asphalt, Gravels, Traffic Control, etc.)	m	\$800	650	\$520,000
1 - 3.5m Wide Median / Turning Lane	m	\$450	100	\$45,000
2.0m Wide Concrete Sidewalk	m	\$280	30	\$8,400
Stormwater main	m	\$425	600	\$255,000
Stormwater catchbasins	qty	\$15,000	15	\$225,000
General Contract Requirements (Mob, Demob, Environmental Controls, Permitting, Coordination)	sum	\$30,000	1	\$30,000
SUBTOTAL				\$1,809,150
Design + Contingencies (30%)				\$542,745
TOTAL				\$2,351,895

Route 329 Improvements	Unit	Unit Cost	Quantity	Price
2 - 0.6m Wide Concrete Curb and Gutter (Concrete Curb, Gravels, Traffic Control, etc.)	m	\$280	1000	\$280,000
1 - 2m Wide Grassed Boulevards	m	\$200	1000	\$200,000
2 - 3.5m Wide Paved Travel Lanes (Asphalt, Gravels, Traffic Control, etc.)	m	\$800	1000	\$800,000
2.0m Wide Concrete Sidewalk	m	\$280	1000	\$280,000
Stormwater main	m	\$425	1000	\$425,000
Stormwater catchbasins	qty	\$15,000	25	\$375,000
General Contract Requirements (Mob, Demob, Environmental Controls, Permitting, Coordination)	sum	\$30,000	1	\$30,000
SUBTOTAL				\$2,390,000
Design + Contingencies (30%)				\$717,000
TOTAL				\$3,107,000

Trail Head and Aspotogan Heritage Trust Placemaking	Unit	Unit Cost	Quantity	Price
Shape Subgrade	sum	\$50,000	1	\$50,000
Pave trailhead parking area	m2	\$1,000	300	\$300,000
Move Rum Runners Trail at 3.5 m width	m2	\$275	120	\$33,000
Sodding allowance	m2	\$18	500	\$9,000
Trees, shrubs, plantings allowance	sum	\$25,000	1	\$25,000
Site signage and furniture	sum	\$100,000	1	\$100,000
General Contract Requirements (Mob, Demob, Environmental Controls, Permitting, Coordination)	sum	\$30,000	1	\$30,000
SUBTOTAL				\$547,000
Design + Contingencies (30%)				\$164,100
TOTAL				\$711,100

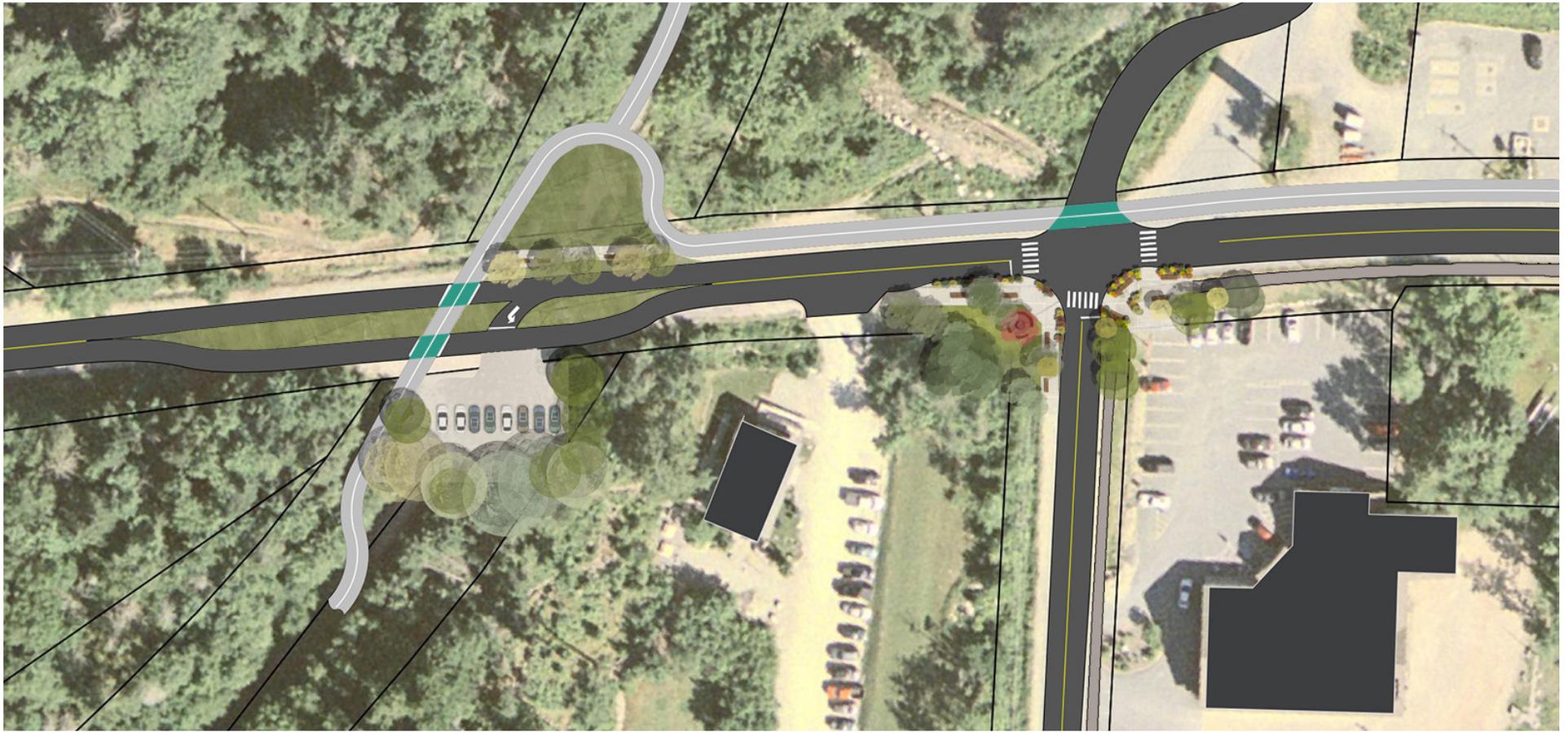


Figure 81: Detail of placemaking locations

14.6 PHASE 5 (OPTIONAL)

The final piece of the project implementation—apart from several objectives which can be achieved through measures described in the Interim Projects section—would be the addition of a southern sidewalk on the stretch of St. Margaret's Bay Road between the Community Waterfront Park and Shankel Road.

This additional walking connection would reduce the need to cross St. Margaret's Bay Road and connect the community of Hubbards to the waterfront in an area which is still underutilized in that respect.

The cost calculation of this phase assumes that the redesign of St. Margaret's Bay Road occurring in Phase 1 would have prepared the site for the installation of a sidewalk. Consequently, costs for realignment of the roadway and curbs are not calculated in this phase.



Figure 82: Map of Phase 5

Transportation Improvements	Unit	Unit Cost	Quantity	Price
1 - 2m Wide Grassed Boulevard	m	\$200	620	\$124,000
2.0m Wide Concrete Sidewalk	m	\$280	620	\$173,600
Stormwater main	m	\$425	620	\$263,500
Stormwater catchbasins	qty	\$15,000	16	\$240,000
General Contract Requirements (Mob, Demob, Environmental Controls, Permitting, Coordination)	sum	\$30,000	1	\$30,000
SUBTOTAL				\$831,100
Design + Contingencies (30%)				\$249,330
TOTAL				\$1,080,430

This final optional phase would also implement the Cove Connections concept through upgrades to Bishop's park and the installation of a dedicated pedestrian bridge crossing Hubbards River, which would frame the view to the front of St. Luke's Church in an iconic way.

Plantings and street furniture improvements within Bishop's Park would also be part of this recommended final phase of the project.



Figure 83: Detail of placemaking locations

Placemaking Improvements	Unit	Unit Cost	Quantity	Price
Trail construction at 3.5 m width	m	\$275	25	\$6,875
Infill for bridge support	m3	\$25	320	\$8,000
Riprap for support	m	\$300	100	\$30,000
Bridge structure	m2	\$3,500	52.5	\$183,750
Sodding allowance	m2	\$18	200	\$3,600
Trees, shrubs, plantings allowance	sum	\$25,000	1	\$25,000
Site furniture - seating, tables, swing hammocks allowance	sum	\$50,000	1	\$50,000
General Contract Requirements (Mob, Demob, Environmental Controls, Permitting, Coordination)	sum	\$30,000	1	\$30,000
SUBTOTAL				\$337,225
Design + Contingencies (30%)				\$101,168
TOTAL				\$438,393

14.7 OVERVIEW

The overall phasing of the implementation of the Hubbards Community Plan results in the following phases and preliminary budget allocations.

Phase 1 is would lay the foundation for active transportation within Hubbards and should ideally be the first part of the project to be

implemented. Changes to the composition and sequence of the other implementation phases may naturally occur and are expected. For example, several multi-unit dwelling buildings along Route 329 are currently in various preparatory stages of development. Should all of these development proposals materialize within a relatively short

period of time, it is possible to fast-track Phase 4 and to implement it before the Cove Connections concept of this plan is completed. Within that concept, Phases 2 and 3 are also largely interchangeable and should occur as opportunities present itself in terms of funding, timing etc.

Phase 5 is the only part of the project implementation which would accomplish optional objectives of the Hubbards Community Plan. However, it is also the phase with by far the smallest budget implications as compared to the other phases.

	Phase Cost	Component Cost
Phase 1	\$7,222,800	
St. Margaret's Bay Road Improvements		\$5,367,960
Library and Waterfront Park Placemaking		\$1,854,840
Phase 2	\$3,955,575	
Fox Point Front Road and Route 329 Sidewalks		\$3,955,575
Phase 3	\$2,707,543	
Shore Club Road Sidewalk and Boardwalk		\$2,707,543
Phase 4	\$6,169,995	
St. Margaret's Bay Road Improvements		\$2,351,895
Route 329 Improvements		\$3,107,000
Aspotogan Heritage Trust Corner / Trailhead		\$711,100
Phase 5 (optional)	\$1,518,823	
St. Margaret's Bay Road Sidewalk		\$1,080,430
Bishop's Park Pedestrian Bridge		\$438,393
GRAND TOTAL PROJECT COST ESTIMATE	\$21,574,735	

